

Environmental Management Plan, Addendum

April 2025
Project 49387-002

G0599 - Second Greater Mekong Subregion Tourism Infrastructure
for Inclusive Growth Project

Vientiane Province, Lao PDR

Boat Docking Facility for the Nam Ngum Reservoir Access
Improvements Subproject

Prepared by the Ministry of Information, Culture and Tourism, Lao PDR, for the Asian Development Bank. This Environmental Management Plan is a document of the borrower. The views expressed herein do not necessarily represent those of ADB's Board of Directors, Management, or staff, and may be preliminary in nature.

CURRENCY EQUIVALENTS

(20 April 2025)

Currency Unit	–	LAK
LAK1.00	=	\$0.000047
\$1.00	=	21,347 LAK

ABBREVIATIONS

DICT	-	Department of Information Culture and Tourism
DPWT	-	Department of Public Works and Transport
DONRE	-	District Office for Natural Resources and Environment
EIA	-	environment impact assessment
ECC	-	environmental compliance certificate
EMP	-	environment monitoring plan
GMS	-	Greater Mekong Subregion
IEE	-	initial environmental examination
EO	-	environmental officer
IUCN	-	International Union for Conservation of Nature
Lao PDR	-	Lao People's Democratic Republic
MICT	-	Ministry of Information, Culture and Tourism
MONRE	-	Ministry of Natural Resources and Environment
MPWT	-	Ministry of Public Works and Transport
PMCES	-	Project Management and Civil Engineering Support Consultant
PIU	-	project implementation unit
PCU	-	project coordination unit
REA	-	rapid environment assessment
UDAA	-	Urban Development and Administration Authority
UXO	-	unexploded ordnance

WEIGHTS AND MEASURES

km:	kilometre
kg:	kilogram
ha:	hectare
mm:	millimetre

NOTE

In this report, "\$" refers to US dollars unless otherwise stated.

Table of Contents

I.	INTRODUCTION	4
II.	CONSTRUCTION WORKS	5
	1. Design and Components	5
	2. Construction Methods	8
III.	ENVIRONMENTAL, HEALTH AND SAFETY IMPACTS	9
IV.	IMPACT MITIGATION PLAN	10
V.	MONITORING PLAN	19
VI.	PUBLIC CONSULTATIONS	19
	1. Consultations during detailed design	19
	2. Consultations Prior to Start of Construction Work	20
	3. Consultations during Construction Works	20
	4. Information Disclosure	20
	APPENDIX A PUBLIC CONSULTATION RECORDS	22

List of Tables

Table 1 Screening of Impacts of the Boat Docking Facility.....	9
Table 2 Impact mitigation plan for the Pre-Construction Period.....	11
Table 3 Impact mitigation plan for the Construction Period	11
Table 4 Impact mitigation plan for the Operating Period	18
Table 5 Environmental monitoring plan	19
Table 6: Consultations during detailed design	20

List of Figures

Figure 1: Plan view of the proposed boat docking facility	6
Figure 2: Typical side view of a mooring bollard.....	7
Figure 3: Side view of a wave attenuator pontoon.....	7
Figure 4: Mooring sinkers	8

I. INTRODUCTION

1. The Second Greater Mekong Subregion Tourism for Inclusive Growth Project (GMS TIIGP) or the Project is a multi-sector tourism development project situated in Lao PDR, Cambodia, and Viet Nam. The project is comprised of transport-related and environmental infrastructure investments in twelve provinces of the three participating countries.
2. The project objective is to help transform secondary GMS central and southern corridor towns into economically inclusive, competitive international tourism destinations by improving road and water transport infrastructure, urban environmental services and capacity to sustainably manage tourism growth
3. In Lao PDR, the Project includes the following five works packages being implemented in Vientiane Province and Champassak Province: Package W01 Nakasang Access Road and Port Rehabilitation (C1) and Don Det-Don Khone Road and Access Improvement (C2), Package W02 Nam Ngum Reservoir Access Improvement (V1), Package W03 Kaeng Yui Waterfall access improvements (V2), and Western Loop Rural Access Road and Bridge Improvements (V3), Package W03a Vang Vieng Urban Renewal subproject (V4), and Package W04: Vang Vieng Solid Waste Management (V5).
4. The Project is financed by a grant from ADB and through financing from the Government of Lao PDR (GOL). The Project is being implemented by the Ministry of Information, Culture and Tourism (MICT) through a Project Coordination Unit (PCU) at central level and Project Implementing Units at provincial level. The Project has contracted a Project Management and Civil Engineering Support Consultant to develop engineering designs, supervise construction works, develop environmental safeguard documents and monitor the implementation of the environmental management plans for each work package.
5. This document is an addendum to the Environmental Management Plan (EMP) developed for Package W02 Nam Ngum Reservoir Access Improvement Subproject, which is based on the Initial Environmental Examination (IEE) for the Subproject (the latest versions of the EMP and IEE are dated April 2025 and the IEE and EMP have been updated in conjunction with this addendum) and all documents including this Addendum to the EMP are available on ADB website).
6. The construction of the Nam Ngum Reservoir Access Improvement Subproject started on 01 July 2021 and all the main works have been completed were completed on 01 December 2024. The completed works include improvements to the existing public marina to safely accommodate 50-60 local tour boats, construction of a circular pavilion, tourist information centre, construction / upgrading of a access roads and parking areas; construction of shop houses, restaurants and market stalls connected to water supply and wastewater treatment and development of public green space.
7. The addendum to the EMP has been developed to cover environmental, health and safety management and mitigation measures for the construction of a boat docking facility under the Nam Ngum Reservoir Access Improvement Subproject.
8. Unless otherwise stated in this Addendum, the provisions and measures in the EMP for the Subproject dated April 2025 shall apply including institutional arrangements, reporting, and grievance management measures.

II. CONSTRUCTION WORKS

1. Design and Components

9. The proposed additional works involve construction of a new boat docking facility to provide safe overnight mooring for 43 tourist boats that will be operating from the marina during the daytime. The boat docking facility is displayed on the plan view in **Figure 1**. Figure 1: Plan view of the proposed boat docking facility

10. The main components include mooring bollards, floating wave attenuator pontoons and mooring sinkers to anchor the floating wave attenuator pontoons.

11. **Mooring bollards.** The contractor shall supply and install 43 bollards on the shoreline for boats to tie on to. The concrete bollards with 25 cm diameter will be installed in excavated pits with the dimensions (width, length, depth) 1.5 m x 1.5 m x 1.1 m (see **Figure 2**). The total amount of soil and rock to be excavated is approximately 106 m³. After installation of the bollards, the pits will be backfilled with excavated materials and crushed rock. An estimated 30 m³ of the excavated soil will be used for backfilling the pits and the remaining 76 m³ shall be safely backfilled on site or brought to a designated spoil disposal site as described in this EMP Addendum **Table 3**.

12. **Floating wave attenuator pontoons.** The Contractor shall supply 15 tanks made of 4 mm epoxy anti-rust painted steel plates with dimensions (width, length, height) 2.0 m x 12.00 m x 1.50 m (see **Figure 3**). The contractor shall install the tanks in two separate rows one with 9 tanks linked together and the other with 6 tanks linked together.

13. **Mooring sinkers.** The Contractor shall supply 34 concrete blocks with the dimensions (width, length, height) 1.2 m x 1.2 m x 1.0 m (1.44 m³) together with mooring ropes to anchor the floating wave attenuator pontoons (see **Figure 4**). Each bloc will weigh about 3.5 tons.

Figure 1: Plan view of the proposed boat docking facility

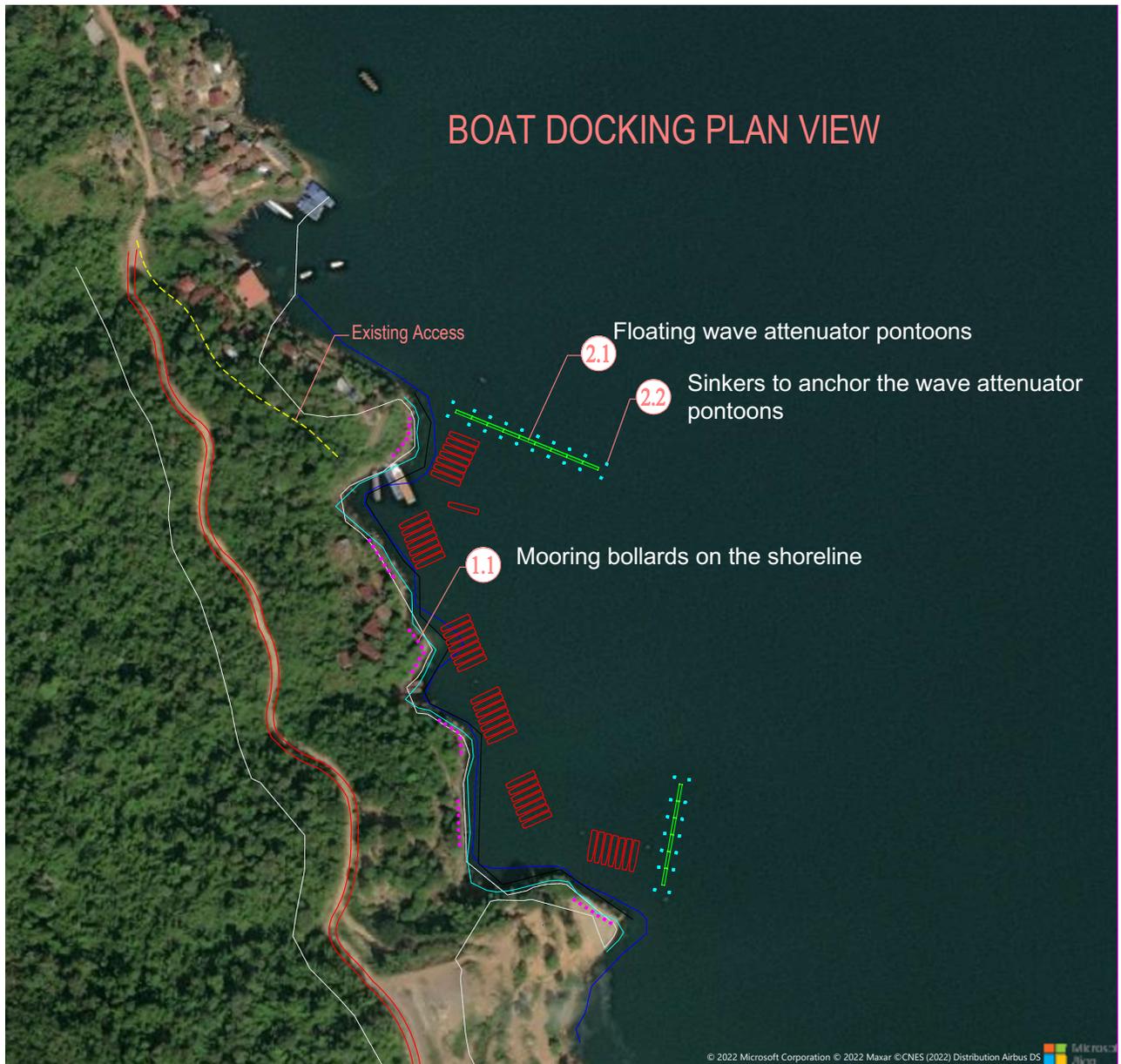


Figure 2: Typical side view of a mooring bollard

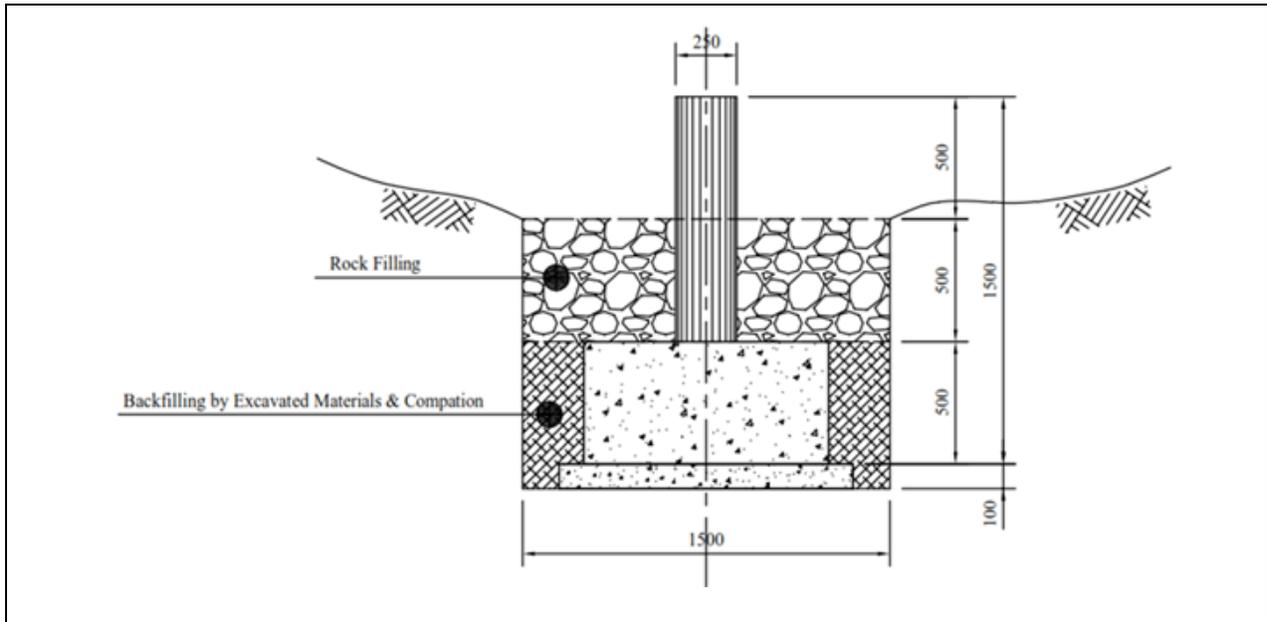


Figure 3: Side view of a wave attenuator pontoon

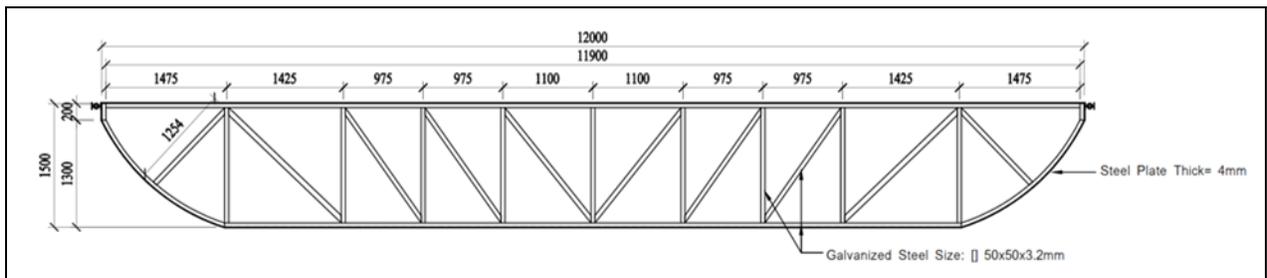
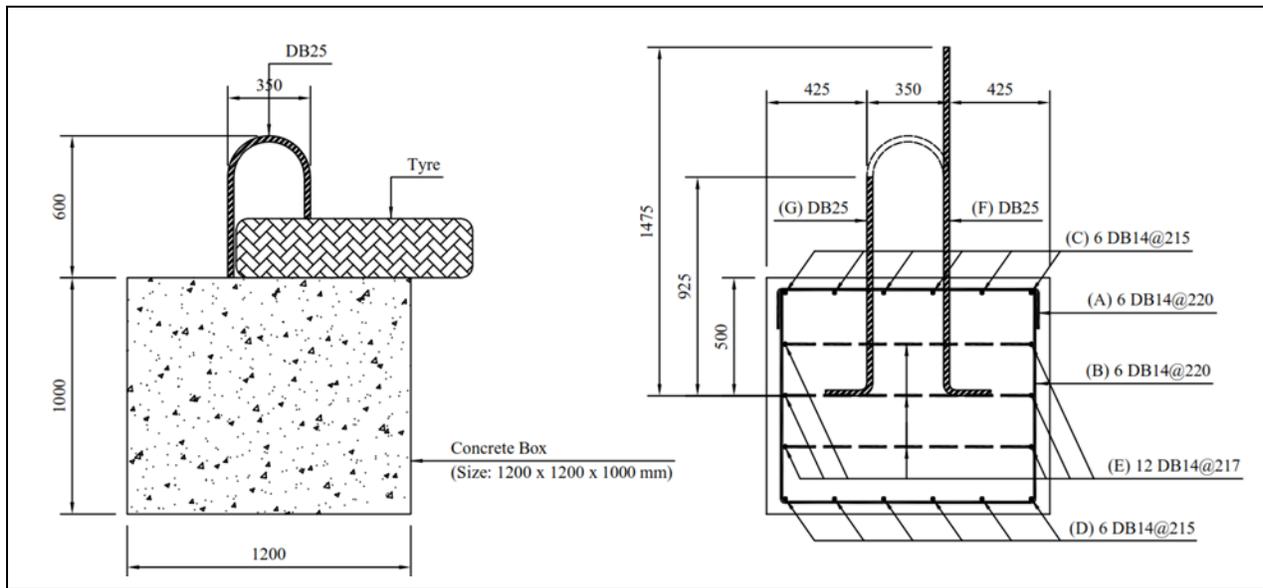


Figure 4: Mooring sinkers



2. Construction Methods

14. The components will be fabricated at existing production facilities under a subcontract with the Contractor or at the Contractor's own facilities. Such facilities shall have the environmental permits or licenses required by Lao Law.
15. As a general rule, all components, materials and equipment shall be transported to the site by boat; however, the Contractor may hand carry minor items/amounts using the existing track from the public road to the mooring site as long as this is done without disturbing any vegetation and without risk of spills.
16. The fabrication of the wave attenuator pontoons will involve steel cutting, welding and painting. Fabrication of concrete blocks and bollards will involve conventional formwork with concrete to be supplied by an existing licensed batching plant.
17. Minor earthworks such as excavation, backfilling and compaction are required to install the mooring bollards on the shoreline.
18. The Contractor is not expected to establish any labour camp. If needed, the Contractor shall use existing office, accommodation and storage facilities.
19. The construction site shall be properly rehabilitated and cleaned up.
20. The construction work is expected be completed within 4 months.

III. ENVIRONMENTAL, HEALTH AND SAFETY IMPACTS

21. The potential environmental, health and safety risks and impacts of the boat docking facility are summarized in **Table 1**.

22. The impact significance is based on a combined assessment of the following factors:

- Spatial extent of the impact (Low: 250 m radius, Moderate: 1 km radius, High 5 km radius)
- Magnitude of the impact (Low: within standards, generally acceptable, Moderate: Slightly above standards but neither acutely nor chronically harmful, High: Exceeding standards with potential to cause acute or chronic harm)
- Duration of the impact (Low: max 4 weeks or max 2 hours per day, Moderate: more than 4 weeks but max 6 months, High: more than 6 months)
- Reversibility of the impact: Low: reversible within one year, Moderate: reversible within 3 years, High: irreversible
- Likelihood of occurring: Low: unlikely, Moderate: Possible, High: Likely

Table 1 Screening of Impacts of the Boat Docking Facility

Risks/Impacts	Sources	Receptors	Impact Significance
Degradation of air quality	<ul style="list-style-type: none"> - Exhaust fumes from construction machinery and boats - Fugitive dust from excavations 	<ul style="list-style-type: none"> - Ambient Air - Residents at nearby house and resort - Workers 	- Low / negligible
Noise nuisance	<ul style="list-style-type: none"> - Noise from construction machinery 	<ul style="list-style-type: none"> - Residents at nearby house and resort - Workers 	- Low / negligible
Impacts on water quality in the reservoir	<ul style="list-style-type: none"> - Discharge of sediment-laden water or water contaminated with cement - Dumping of spoils in the reservoir - Accidental fuel / oil spills from boat operations or use of equipment 	<ul style="list-style-type: none"> - Reservoir water and aquatic organisms 	- Low with the mitigation measures in place
Destruction of fauna and flora	<ul style="list-style-type: none"> - Clearing of vegetation for installation of mooring poles at the shoreline 	<ul style="list-style-type: none"> - Trees, bushes and shrubs 	- Low with the mitigation measures in place
Illness	<ul style="list-style-type: none"> - Exhaust fumes from construction machinery and equipment - Fugitive dust from and all excavation works - Inhalation of welding or plasma cutting fumes - Inhalation of paint fumes 	<ul style="list-style-type: none"> - Workers 	- Low with the mitigation measures in place
Injuries, fatal accidents	<ul style="list-style-type: none"> - Lifting operations - Struck by moving equipment - Slips and falls - Falls into the reservoir with risk of drowning 	<ul style="list-style-type: none"> - Workers - Boat operators and their staff 	- Moderate with the mitigation measures in place

Risks/Impacts	Sources	Receptors	Impact Significance
	- Burns or cuts from welding or cutting		

IV. IMPACT MITIGATION PLAN

23. The Impact Mitigation Plans for the construction and operation of the boat docking facility are presented in **Table 2**, **Table 3** and **Table 4**.

Table 2 Impact mitigation plan for the Pre-Construction Period

Pre-Construction Subproject Activity	Potential Environmental Impacts	Mitigation Measures Pre-Construction Period	Supervision	Implementation
Consultations and disclosure of safeguard documents	To prevent impacts and grievances	Disclosure of the EMP, Grievance Redress Mechanism and construction activities and schedules to affected people and businesses	PCU	PIU/PMCES
Government liaison	To prevent risk of non-compliance	Submit the EMP to DONRE for their information	PCU	PIU/PMCES
Contractor's selection of fabrication sites / suppliers	Risk of poor environmental management leading to environmental impacts	Contractor to prepare a method statement for the works identifying the fabrication sites or suppliers and documenting possession of required permits/certificates	PCU/PMCES	Contractor
Develop bid documents	Prevent or minimize impacts	Ensure that this updated EMP is included in construction tender documents, and that tender documents specify requirements for delivery of a construction method statement with site specific environment, health and safety measures in accordance with this EMP and the full EMP for the subproject	PCU	PMCES

Table 3 Impact mitigation plan for the Construction Period

Construction Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Construction Period	Supervision	Implementation
Labour management Workers' accommodation and welfare facilities	Unhealthy workplace or facilities for workers Pollution and social problems	<ol style="list-style-type: none"> 1. The Contractor shall use existing accommodation, welfare and office facilities for workers and other staff for the construction work. Such facilities shall be in compliance with the ILO Labour Housing Standards 2. Document compliance with the Labour Law No. 43/NA dated 24 December 2013 and the Law on Social Security No. 54/NA dated 27 June 2018 3. Ensure and document compliance with minimum age of 18 years old for construction workers. 4. Provide health checks for the workers in accordance with the Labour Law. 	PMCES/PIU	Contractor

Construction Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Construction Period	Supervision	Implementation
On site sanitation and welfare facilities	Unhealthy workplace or facilities for workers Pollution	5. Boats used for the construction work shall be equipped with appropriate sanitation facilities with no discharge of sanitary wastewater to the reservoir 6. Sanitary facilities and other areas where workers may eat, or rest shall be kept clean at all times 7. Sanitary facilities shall have adequate supply of soap and tissue paper. 8. Boats shall have place for eating and resting for the workers. 9. Unless easy access to the toilets on the boat(s) is provided, portable toilets shall be provided for workers working along the shoreline. The toilets shall be regularly emptied with the content being transported to the wastewater treatment system at the marina. All workers must be instructed to use these facilities, which shall always be kept clean.	PMCES/PIU	Contractor
Training	Prevention of impacts through education	10. The Contractor shall carry out training of the construction workers, boat operators and site engineers/managers on occupational health and safety, and prevention of environmental impacts Implement including the relevant measures in this EMP	PMCES/PIU	Contractor
Construction materials transport, and storage	Water pollution, disturbance of shoreline vegetation	11. As a general rule, all components, materials and equipment shall be transported to the site by boat; however, the Contractor may hand carry minor items/amounts using the existing foot track from the public road to the mooring site as long as this is done without disturbing any vegetation and without risk of spills.	PMCES/PIU	Contractor
Excavations for mooring bollards	Water pollution with sediment-laden runoff, disturbance of vegetation	12. As a general rule, all excavations shall be carried out manually or with the use of handheld powered equipment. If the Contractor wishes to use other types of equipment, the Contractor shall submit a written proposal with description of the equipment and how it will be transported, fuelled and operated to PMCES for consideration and approval prior to the intended use. 13. Excavated soil shall be stockpiled on site and used for backfilling of the pits and rehabilitation of the worksite. 14. Any excess excavated soil or rock shall to the extent practicable be used for manual backfilling of existing disturbed depressions with no or sparse vegetation above the high-water mark at the worksite. The backfilled materials shall be compacted. 15. Prior to backfilling of excess soil, the Contractor shall ensure approval by PMCES of the places identified for backfilling. 16. No bushes or trees may be cut or removed. 17. Burning of vegetation is prohibited. 18. In case there are no suitable places for backfilling with excess excavated soil at the construction site, the Contractor shall remove the soil and dispose of	PMCES/PIU	Contractor

Construction Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Construction Period	Supervision	Implementation
		the soil at a designated spoil disposal site approved by PIU, DONRE and PMCES.		
Use of portable concrete mixer	Risk of water pollution from uncontrolled disposal of concrete waste Risk of fire from improper handling of fuel	<p>19. The work areas shall clear of debris and obstructions. The mixer shall be on a flat and level surface.</p> <p>20. Before using, check to determine that the mixer will operate properly. Any broken or missing parts must be repaired or replaced before using. Check for proper machine guarding and assure the electrical power cord is in good condition.</p> <p>21. Use Ground Fault Circuit Interrupter protection when using an electric powered mixer outdoors or in damp locations to prevent a shock/fire hazard.</p> <p>22. For petrol fuel powered mixers, always shut off engine before fuelling.</p> <p>23. The mixer when operating must not be left unattended.</p> <p>24. Clean the mixer in a designated area where concrete and rinse water can be contained with no discharge to the reservoir and left to evaporate and seep into the ground. Any leftover hardened concrete shall be backfilled together with any excess soil and covered by the soil.</p>	PMCES/PIU	Contractor
Management of solid and liquid waste	Water pollution, risk of fire or explosion	<p>25. Sufficient waste containers shall be available at the worksite on the shore and on-board for temporary storage of solid and liquid waste.</p> <p>26. Construction waste shall be collected and segregated daily, temporarily stored in tight and closed containers at the boat or on the shore and non-hazardous, non-recyclable solid shall be disposed of at a site approved by DONRE.</p> <p>27. Recyclables shall be separated and given/sold to recyclers (plastic, metal, card, paper as a minimum).</p> <p>28. Burning of waste is prohibited.</p>	PMCES/PIU	Contractor

Construction Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Construction Period	Supervision	Implementa- tion
Management of hazardous materials and hazardous waste	Risk of water pollution from spills Risk of fire or explosion by ignition of spilled or leaking fuel	29. Areas where spills of fuel or oil may occur shall be equipped with easily accessible spill control kits to assist in prompt and effective spill control. 30. Stationary equipment such as motors, pumps, generators shall be positioned over drip pans 31. Any hazardous waste (oil waste) shall be properly collected and stored in closed container under shelter for reuse/recycling or disposal, to ensure they will not contaminate soil or surface water. 32. Before and during refuelling, the motor shall be turned off. 33. It is prohibited to smoke or use open flames within 15 m of a refuelling operation or where fuel is stored. 34. The equipment shall be allowed to cool off before refuelling. 35. Both fuel dispensing tank and the equipment shall be rounded. 36. Ensure that a fire extinguisher is always available 7 m of the refuelling. 37. Areas where spills of fuel or oil may occur shall be equipped with easily accessible spill control kits to assist in prompt and effective spill control. 38. All spills must be cleaned up completely with all contaminated soil removed and handled as contaminated soil.	PMCES/PIU	Contractor
Rehabilitation	Risk of erosion, sediment-laden runoff, pollution from spills or waste	39. Upon completion of construction works, the Contractor shall clean-up the construction site, remove all construction waste and ensure disposal of the any waste or spills as required under Item 26 to Item 38 . 40. To the extent practicable, the construction site shall be rehabilitated to its pre-construction works condition with no risk of excess erosion. 41. Revegetation of disturbed or backfilled areas shall be promoted by placing a layer or topsoil on the backfill and planting grass.		
All construction works	Risks to workers' health and safety	42. Undertake hazard and risk assessment of planned work. 43. prepare a health and safety plan containing site-specific precautions to properly address the identified hazards and risks in accordance with relevant occupational health and safety guidelines. 44. inspect and check the relevant construction equipment to ensure that it meets the applicable mechanical and safety requirements. 45. inspect the worksite to ensure that the equipment can be safely mobilized and operated, and that there are no unmitigated risks. 46. carry out daily toolbox meetings (safety briefings). 47. maintain an accident record book where all major or minor accidents and incidents are recorded with actions taken. 48. educate the workers on construction hazards. 49. train boat and plant operators on safety measures. 50. appoint an Environment, Health and Safety Officer who is a qualified engineer. 51. make adequate first aid equipment available on site.	PMCES/PIU	Contractor

Construction Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Construction Period	Supervision	Implementation
		52. implement emergency preparedness and response procedures (to be incorporated in the Method Statement).		
Working on boats or workplatforms on water	Risks to workers' health and safety Risk of drowning or serious injuries	53. Lifebuoys shall be available at the vessels or floating work platforms at all times with buoyant lifeline of adequate length attached. 54. Reasonable personal buoyancy equipment, such as life jackets, should be provided by employers 55. Boats or other work platforms used for work on water shall be equipped with appropriate guard rails to prevent and safeguard falling into water, e 56. Appropriate safety footwear to minimize the risk of slipping. 57. Boats and work platforms on water shall have guardrails to prevent falls into water and drowning. 58. Always work in teams consisting of minimum three workers within eyesight of each other. 59. Wear life west when using boats or when working on floating platforms. 60. Ensure that waterproofed communication devices are available. 61. Wear long sleeved shirt and full-length trousers. 62. Use insect repellent.	PMCES/PIU	Contractor
Welding and oxy-fuel or plasma cutting	Risks to workers' health and safety Risk of burns, radiation injuries, respiratory diseases	63. Use proper eye protection such as welder goggles and/or a full-face eye shield for all personnel involved in, or assisting, welding or cutting operations. 64. If there is any risk of other people being exposed to the bright light from welding or cutting then use a solid piece of light metal, canvas, or plywood to block welding light from others. 65. Ensure good ventilation.	PMCES/PIU	Contractor
Use or handling of hazardous materials	Risks to workers' health and safety	66. Maintain a Safety Data Sheet for each chemical used/stored at the construction site. 67. Inform the workers and employees about the content of the Safety Data Sheets, the hazards and risks. 68. Train the workers and employees on how to read and use the Safety Data Sheets 69. Follow the instructions in the Safety Data Sheet for storage, use and handling hazardous materials. 70. Provide spill clean-up kits in areas where hazardous chemicals are stored or handled. 71. Train employees to clean up spills, protect themselves and properly dispose of used materials.	PMCES/PIU	Contractor

Construction Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Construction Period	Supervision	Implementation
		72. Provide proper personal protective equipment in accordance with the Safety Data Sheets and enforce its use.		
Use of electrical equipment	Risks to workers' health and safety Risk of electrocution, electric shock and burns	73. Damaged or worn electrical cords or cables shall promptly be replaced. 74. All extension cords shall have grounding prongs. 75. Protect flexible cords and cables from damage. Sharp corners and projections should be avoided. 76. Use extension cord sets used with portable electric tools and appliances that are the three-wire type and designed for hard or extra-hard service. 77. All electrical tools and equipment shall be maintained in safe condition and checked regularly for defects and taken out of service if a defect is found. 78. Overhead electrical power lines shall be located and identified. 79. All electrical tools must be properly grounded unless they are of the double insulated type. 80. Multiple plug adapters may not be used.	PMCES/PIU	Contractor
Servicing and maintaining machines	Risks to workers' health and safety Risk of severe injuries or fatal accidents	81. Practices and procedures shall be put in place to safeguard workers from the unexpected energization or start-up of machinery and equipment, or the release of hazardous energy during service or maintenance activities. 82. Before performing service or maintenance on machinery or equipment, all power must be turned off and the machinery or equipment must be disconnected from its energy source(s). 83. A duly authorized employee shall lock or tag the energy-isolating device(s) to prevent the release of hazardous energy and take steps to verify that the energy has been isolated effectively	PMCES/PIU	Contractor
Use or handling of compressed gas cylinders	Risks to workers' health and safety Risk of fire or explosion, serious injuries from struck by heavy cylinders	84. Limit access to cylinder storage area to authorized personnel. 85. Keep cylinders vertical. 86. Secure cylinders with straps or chains connected to a wall bracket or other fixed surface, or by use of a cylinder stand to prevent falling. 87. Full cylinders should be stored separate from empty cylinders. 88. Cylinder storage area shall be ventilated, and kept clean and dry. 89. Do not let indoor temperature exceed 38°C. 90. Use a hand truck or cart designed to move cylinders. 91. Keep cylinder vertical, except for short periods of time while cylinders are being hoisted or carried. 92. Oxygen cylinders in storage shall be separated from fuel-gas cylinders or combustible materials (especially oil or grease) by a minimum distance of 6 m or by a non-combustible barrier at least 1.5 m tall having a fire-resistant rating of at least one-half hour	PMCES/PIU	Contractor

Construction Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Construction Period	Supervision	Implementation
Lifting Operations	Risks to workers' health and safety Risk of serious injuries or fatal accidents from falling objects or slips and falls	93. The Contractor shall make sure that the lifting equipment is appropriate and certified for the lifting operations and that it is intact, fully functional and maintained in accordance with the manufacturer's instructions. 94. Before starting a lifting operation, the Contractor shall prepare a risk assessment. 95. Based on the risk assessment, the Contractor shall prepare a lifting plan and appoint the lifting crew composed of competent and experienced personnel. 96. The lifting crew shall be instructed in the lifting plan and its health and safety precautions. 97. One crew member shall act as the signal-person. The equipment operator shall take directions from that person only. 98. Keep hands clear of any pinch points. 99. Use tag lines to control the load. 100. Ensure that no one stand under a raised load. 101. Crew members shall position themselves, so they don't get caught in between the load and a stationary object. 102. Create an exclusion zone in case stacked material is knocked over by a swinging load. 103. Remove unnecessary objects from where the load will be placed. 104. A load must never be left suspended in the air. 105. It is strictly forbidden to ride on the equipment or load.	PMCES/PIU	Contractor
All construction works	Risk of workplace injuries	106. Standard mandatory PPE includes: <ul style="list-style-type: none"> - hard hat - coveralls with long sleeves and full-length pants - high visibility clothing in yellow or orange material with reflective panels. - safety boots with metal toe cap. 107. Work specific PPE includes: <ul style="list-style-type: none"> - Cut-resistant work gloves (working with waste). - Respirator / face mask when working in areas where waste is excavated, loaded or otherwise agitated. - Ear protection (earplugs or muffs) wherever it is not feasible to reduce the noise levels or duration of exposures to those specified in internationally recognized guidelines and in any case where the noise level is 80 dB(A) or higher. - Safety glasses (waste handling, crushing/grinding, cutting). - Welding hoods with clear safety glasses under (welding). - Life vest for workers when using a boat or working on a floating platform. 	PMCES/PIU	Contractor

Table 4 Impact mitigation plan for the Operating Period

Operating Period Subproject Activity	Potential Environmental, Health and Safety Impacts	Mitigation Measures Operating Period	Supervision	Implementation
Maintenance of mooring facilities	Slips and falls due to damaged facilities, poor maintenance	<ol style="list-style-type: none"> 1. Check for any visible signs of damage as part of the daily routine. 2. Ensure that any slip hazards are removed straight away, including washing and removing excessive plant growth that may be a slip hazard. 3. Conduct thorough inspections of the pontoons. 4. Fix and repair even small damages as soon as possible. 5. During storms, ensure that the pontoons are made as secure as possible. Ensure that the pontoons are inspected immediately after storms for damage 	Management Company	Boat Association
Working on boats	<p>Health and safety risks to boat operators and their staff</p> <p>Risk of drowning or serious injuries</p>	<ol style="list-style-type: none"> 6. Lifebuoys shall be available at the vessels at all times with buoyant lifeline of adequate length attached. 7. Reasonable personal buoyancy equipment, such as life jackets, should be provided by the boat operators 8. Boats shall be equipped with appropriate guard rails to prevent and safeguard falling into water, 9. Use appropriate safety footwear to minimize the risk of slipping. 10. Boats and work platforms on water shall have guardrails to prevent falls into water and drowning. 11. Always work in teams consisting of minimum three workers within eyesight of each other. 12. Wear life west when using boats. 13. Ensure that waterproofed communication devices are available. 14. Wear long sleeved shirt and full-length trousers 	Boat Association	Boat operators

V. Monitoring Plan

24. Due to the short duration of the construction works and the relatively minor anticipated environmental impacts, the environmental monitoring to be carried out under this EMP Addendum is limited to regular site inspections by PMCES and PIU and to the Contractor's own site control and inspection as summarized in **Table 5**.

25. Note that monitoring and inspection during the operating period are included in the main Environmental Management Plan for the Nam Ngum Reservoir Access Improvement subproject (latest version of April 2025).

Table 5 Environmental monitoring plan

Parameters / Indicators	Location	Means of Monitoring	Frequency	Implementation by	Supervision by
Water quality in the reservoir: Extent of visibly increased turbidity due to construction activities	Near shoreline for the boat docking facility	Visual inspection	Daily during construction of mooring bollards	Contractor	PMCES
Dust: Extent of visible dust from construction activities	At the construction site for the mooring bollards and at the nearest house	Visual inspection	As above	Contractor	PMCES
Noise: Nuisance noise at the site and at the nearest house	At the construction site for the mooring bollards and at the nearest house	Human ear	During use of noisy equipment	Contractor	PMCES
Health and safety precautions in place	At the construction site for the mooring bollards	Physical inspection	Daily immediately before and during installation	Contractor	PMCES
Health and safety precautions in place for working on water	Boats / floating platforms for installation of pontoons and sinkers	Physical inspection	As above	Contractor	PMCES

VI. PUBLIC CONSULTATIONS

1. Consultations during detailed design

26. Detailed consultations have been held with members of the tour boat association on the design and layout of the boat dock facility, particularly the location of boat mooring bollards and the location of wave break pontoons. On 30 August 2024 a stakeholder meeting was held to review and confirm the final design for the boat dock. During these stakeholder consultations, information was presented on the proposed design, scope of works, the

safeguards screening, and the Grievance Redress Mechanism. A summary of consultations is provided in **Table 6** and consultation records are in Appendix A.

Table 6: Consultations during detailed design

Date	Venue	Participants	Summary of main topics and discussion
30/08/2024	Nam Ngum Marina	<ul style="list-style-type: none"> • Vice District Governor • Provincial Tourism Dept, • District Tourism Dept, • PCU safeguard consultants • Boat association members • Nearby concession holders • Total: 18 (female 4) 	<ul style="list-style-type: none"> • Presentation of proposed design. • Agreed reduction in scope to meet budget ceiling (exclude access road). • Agreed to expedite civil works to provide permanent mooring. • Agreed that tourist boats will not be permitted to moor at the tourist marina as this would block the view for visitors. • Confirmed there will not be any impact on existing land rights or land use. • Confirmed the public access track has a right of way 6 m wide. • Agreed that future (new) tourist boat operators will also have the right to use the dock.
23/12/2024	Nam Ngum Marina	<ul style="list-style-type: none"> • PIU • District Tourism Dept, • PCU safeguard consultants • Boat Association members • Total: 10 (female 3) 	<ul style="list-style-type: none"> • Presentation of detailed engineering design • Design agreed and accepted by members of the Boat Association.

2. Consultations Prior to Start of Construction Work

27. Prior to undertaking the works, the PIU in cooperation with PCU and PMCES will meet and consult with the owners/users of land adjacent to construction site and with boat owners and operators to inform them about the upcoming work, work schedules and to hear if they have any concerns or recommendations.

3. Consultations during Construction Works

28. During implementation of the construction works, the PIU and PMCES safeguard specialists and the contractor will meet and consult with owners/users of land adjacent to construction site and with boat owners and operators to hear if they have any concerns or recommendations Post Closure Public Consultations

4. Information Disclosure

29. The following environmental safeguard documents will be disclosed in accordance with ADB's Public Communications Policy (2011) and SPS (2009):

- The EMP Addendum is available for review at PIU office;
- The full IEE and EMP are disclosed on ADB's project website;
- Copies of the EMP Addendum are available upon request; and

- Semi-annual environmental and social monitoring reports covering the GMS TIIGP in Lao PDR with information about compliance with the relevant EMP for each subproject and other necessary information will be available on ADB's project website.

APPENDIX A PUBLIC CONSULTATION RECORDS

B. Public Consultation Records

1. Agreement for location of the Dock, March 2022



ສາທາລະນະລັດ ປະຊາທິປະໄຕ ປະຊາຊົນລາວ
ສັນຕິພາບ ເອກະລາດ ປະຊາທິປະໄຕ ເອກະພາບ ວັດທະນະຖາວອນ

ແຂວງວຽງຈັນ
ເມືອງແກ້ວອຸດົມ

ເລກທີ. 14. / ຈມ.ກດ
ລົງວັນທີ 16/03/2022

ຂໍ້ຕົກລົງ

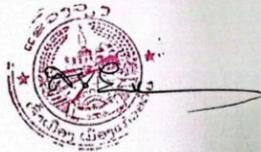
ວ່າດ້ວຍ ການກຳນົດທາງລົງທຳເຮືອ ແລະ ສະຖານທີ່ຈອດເຮືອຖາວອນຂອງລັດຄຸ້ມຄອງ
ຢູ່ຫນ້າເຂື່ອນນ້ຳຖິ່ມ 1 (ໂສກປາແກ້ວ) ບ້ານແສງສະຫວ່າງ ເມືອງແກ້ວອຸດົມ

- ອີງຕາມ ກົດໝາຍປົກຄອງທ້ອງຖິ່ນ ສະບັບເລກທີ 68/ສພຊ, ລົງວັນທີ 14 ທັນວາ 2015.
- ອີງຕາມ ກົດໝາຍວ່າດ້ວຍທີ່ດິນ ສະບັບເລກທີ 70/ສພຊ, ລົງວັນທີ 21 ມິຖຸນາ 2019.
- ອີງຕາມ ໜັງສືສະເໜີຂອງກຸ່ມເຮືອນຳທ່ຽວອ່າງນ້ຳຖິ່ມ 1 ສະບັບເລກທີ 01/ກຮທ, ລົງວັນທີ 16 ມັງກອນ 2022.

ເຈົ້າເມືອງຕົກລົງ:

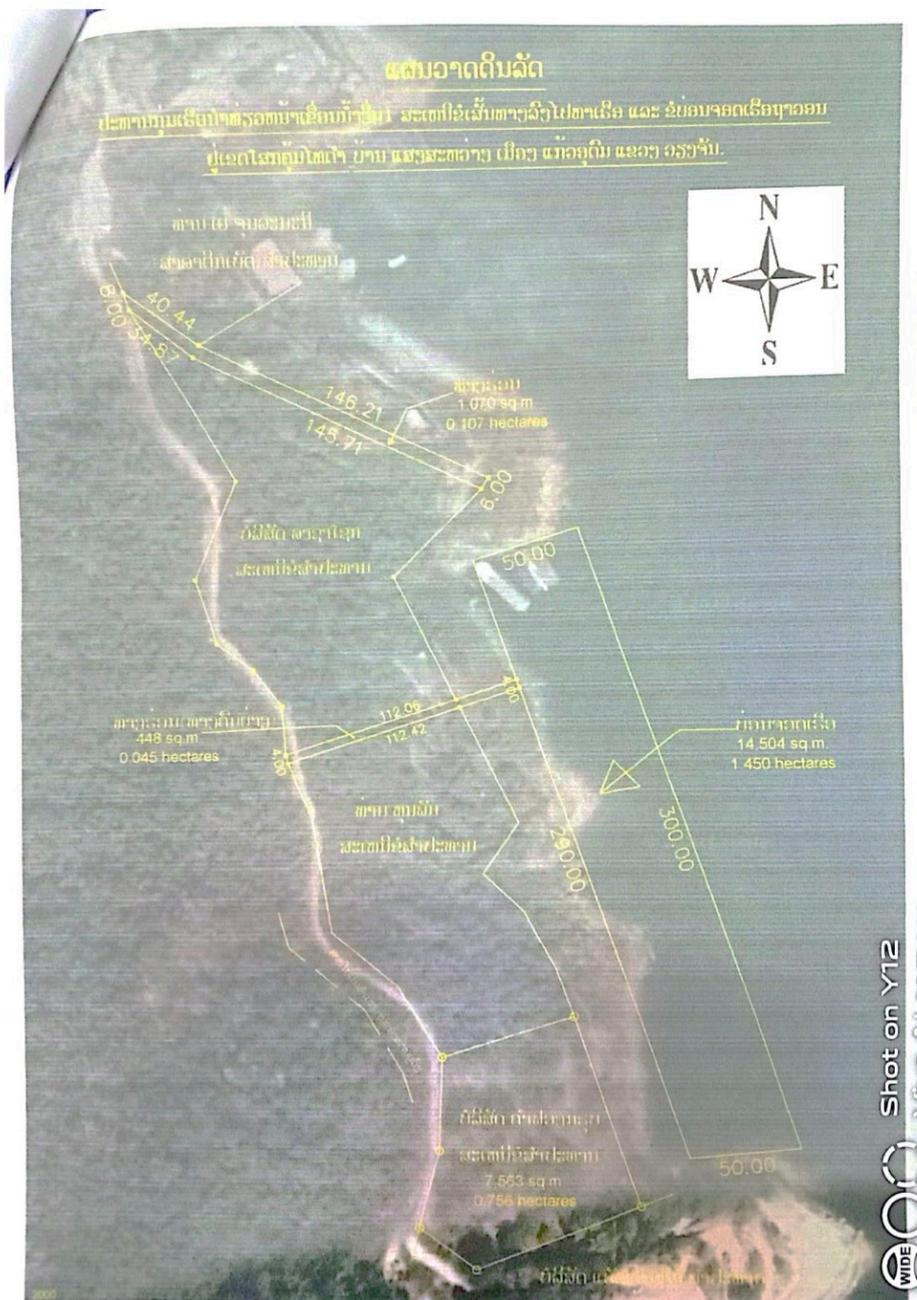
- ມາດຕາ 1 ຕົກລົງກຳນົດຂອບເຂດທາງລົງທຳເຮືອ ເຊິ່ງມີຄວາມກວ້າງ 6 ແມັດ, ຍາວ 186,65 ແມັດ, ລວມເນື້ອທີ່ທັງໝົດ 1,070 ຕາແມັດ ຫຼື ເທົ່າກັບ 0,107 ເຮັກຕາ ແລະ ທາງຢາງລົງທຳເຮືອລະຫວ່າງເຂດແດນທ່ານ ຫຸ້ມພັນ ສະເໜີຂໍສຳປະທານ ແລະ ບໍລິສັດລາຊາໂຊກສະເໜີຂໍສຳປະທານເບື້ອງລະ 2 ແມັດ ເຊິ່ງລວມຄວາມກວ້າງ 4 ແມັດ, ຍາວ 112,42 ແມັດ, ລວມເນື້ອທີ່ທັງໝົດ 448 ຕາແມັດ ເທົ່າກັບ 0.045 ເຮັກຕາ.
- ມາດຕາ 2 ກຳນົດຂອບເຂດທຳຈອດເຮືອທີ່ລັດຄຸ້ມຄອງ ໂດຍມອບໃຫ້ກຸ່ມເຮືອນຳທ່ຽວເປັນຜູ້ນຳໃຊ້ ເພື່ອເປັນບ່ອນຈອດເຮືອຖາວອນຢູ່ຫນ້າເຂື່ອນນ້ຳຖິ່ມ 1 ເຊິ່ງມີຄວາມກວ້າງ 50 ແມັດ, ຍາວ 290 ແມັດ ເບື້ອງຫນ້າຕິດກັບຕອນດິນ ແລະ ເບື້ອງຫຼັງຕິດກັບນ້ຳຖິ່ມມີຄວາມຍາວ 300 ແມັດ, ລວມເນື້ອທີ່ທັງໝົດ 14,504 ຕາແມັດ, ເທົ່າກັບ 1,450 ເຮັກຕາ ໂດຍມອບໃຫ້ໂຄງການ ADB ເປັນຜູ້ພັດທະນາ, ຫຼັງຈາກພັດທະນາສຳເລັດແລ້ວແມ່ນ ມອບຄືນໃຫ້ກຸ່ມເຮືອນຳທ່ຽວເປັນຜູ້ນຳໃຊ້ຕາມລະບຽບການ.
- ມາດຕາ 3 ສຳລັບພື້ນທີ່ດັ່ງກ່າວ ຫ້າມເອົາໄປຈັດສັນໃຫ້ພະນັກງານຄຸ້ມຄອງນຳໃຊ້, ແລກປ່ຽນ, ຄ້າປະກັນ, ໂອນສິດ ຫຼື ຊື້-ຂາຍ ໃຫ້ບຸກຄົນ, ນິຕິບຸກຄົນ, ການຈັດຕັ້ງຂອງລັດ ແລະ ຫ້າມນຳໃຊ້ໄປໃນທາງທີ່ຜິດຕໍ່ກົດໝາຍ.
- ມາດຕາ 4 ໃຫ້ການຈັດຕັ້ງຂອງລັດ, ພາກສ່ວນທີ່ກ່ຽວຂ້ອງຈົ່ງຮັບຮູ້ ແລະ ຈັດຕັ້ງປະຕິບັດຕາມຂໍ້ຕົກລົງສະບັບນີ້ຢ່າງເຂັ້ມງວດ.
- ມາດຕາ 5 ຂໍ້ຕົກລົງສະບັບນີ້ ມີຜົນສັກສິດ ແລະ ນຳໃຊ້ໄດ້ ນັບແຕ່ວັນລົງລາຍເຊັນເປັນຕົ້ນໄປ.

ຜູ້ຮອງ ເຈົ້າເມືອງ



ສິງຄຳ ມະນີວັນ

Shot on Y12
VIVO AI camera
WIDE

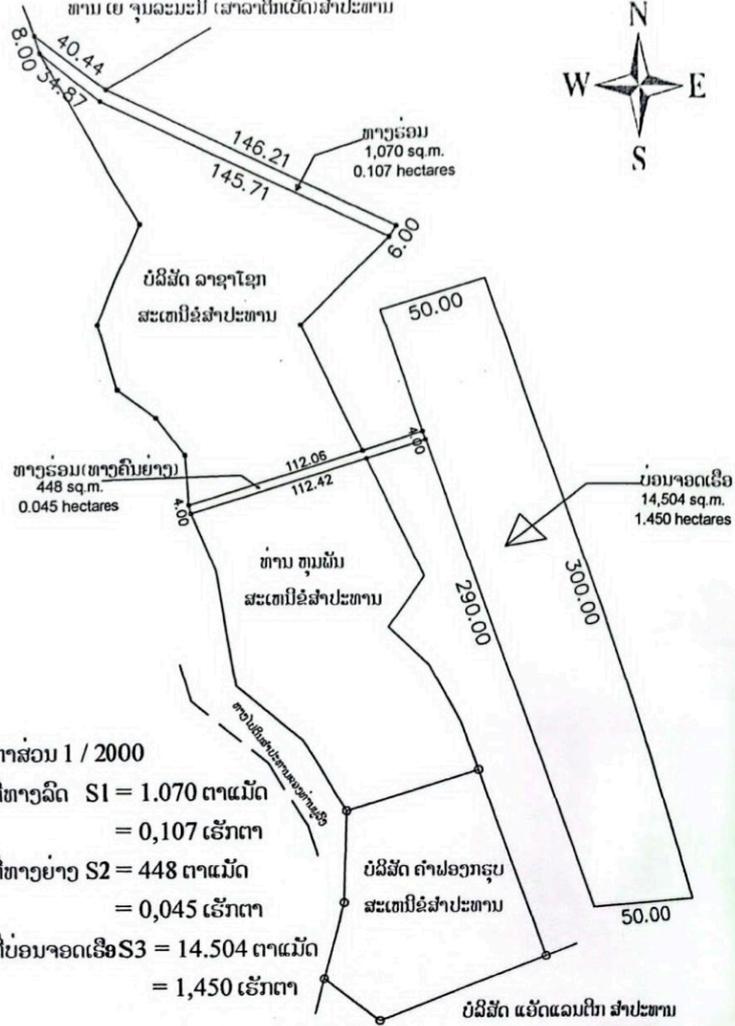


ສາທາລະນະລັດ ປະຊາທິປະໄຕ ປະຊາຊົນລາວ
 ສັນຕິພາບ ເອກະລາດ ປະຊາທິປະໄຕ ເອກະພາບ ວັດທະນາຖາວອນ

ແຜນວາດດິນລັດ

ປະທານກຸ່ມເຮືອນຳທ່ຽວສຳເລັດເຊື້ອນ້ຳຢູ່ນ້ຳ ສະເຫນີຂໍເສັ້ນທາງລົງໄປຫາເຮືອ ແລະ ຂໍບ່ອນຈອດເຮືອຖາວອນ
 ຢູ່ເຂດໂສກຄຸ້ມໂຫດຳ ບ້ານ ແສງສະຫວ່າງ ເມືອງ ແກ້ວອຸດົມ ແຂວງ ວຽງຈັນ.

ທ່ານ ເຍ ຈຸນລະນະນິ (ສາລາຕິນເບັດ) ສຳປະທານ



ມາດຕາສ່ວນ 1 / 2000
 ເນື້ອທີ່ທາງລົດ S1 = 1.070 ຕາແມັດ
 = 0,107 ເຮັກຕາ
 ເນື້ອທີ່ທາງຍ່າງ S2 = 448 ຕາແມັດ
 = 0,045 ເຮັກຕາ
 ເນື້ອທີ່ບ່ອນຈອດເຮືອ S3 = 14.504 ຕາແມັດ
 = 1,450 ເຮັກຕາ

Shot on Y12

2. Stakeholder Consultation on Final Design, August 2024



ສາທາລະນະລັດ ປະຊາທິປະໄຕ ປະຊາຊົນລາວ
ສັນຕິພາບ ເອກະລາດ ປະຊາທິປະໄຕ ເອກະພາບ ວັດທະນະຖາວອນ

ບົດບັນທຶກ

ກອງປະຊຸມປຶກສາຫາລື ກ່ຽວກັບ ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນ ຂອງໂຄງການ(ADB)
ຢູ່ອ່າງນ້ຳຼືມ ເມືອງແກ້ວອຸດົມ ແຂວງວຽງຈັນ

ໃນຕອນແລງເວລາ 13:30 ໂມງ ຂອງວັນທີ 30/8/2024 ທີ່ຫ້ອງປະຊຸມ ຫ້ອງການຍຸຕິທຳເມືອງແກ້ວອຸດົມ ໄດ້ຈັດກອງປະຊຸມປຶກສາຫາລື ກ່ຽວກັບ ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນ ຂອງໂຄງການ(ADB) ຢູ່ອ່າງນ້ຳຼືມ ເມືອງແກ້ວອຸດົມ, ໂດຍໃຫ້ກຽດເປັນປະທານຮ່ວມຂອງ ທ່ານ ສີສະຫວາດ ແສງພະຈັນ ຫົວໜ້າພະແນກຖວທ ແຂວງວຽງຈັນ ແລະ ທ່ານ ສິງຄຳ ມະນີວັນ ຮອງເຈົ້າເມືອງແກ້ວອຸດົມ, ມີທີມງານຊຽວຊານປົກປ້ອງສັງຄົມ, ຄະນະໄກ່ເກ່ຍ, ສະມາຊິກກຸ່ມເຮືອນ້ຳທ່ຽວອ່າງນ້ຳຼືມ ແລະ ຫົວໜ້າພະແນກກົດເຊົ່າ-ສຳປະທານທີ່ດິນ ເຂດໄກ່ຄຽງກັບພື້ນທີ່ຈະກໍ່ສ້າງທ່າເຮືອຖາວອນຂອງໂຄງການຢູ່ເຂດໂສກປາແກ້ວ-ໂສກໄທດຳ ເຂົ້າຮ່ວມ ດັ່ງມີລາຍຊື່ຄັດຕິດມາພ້ອມນີ້.

ໃນກອງປະຊຸມ ກໍ່ໄດ້ຮັບຟັງການລາຍງານສະພາບລວມ ແລະ ຄວາມຈຳເປັນຂອງການທ່າເຮືອຖາວອນຂອງໂຄງການ, ຊຶ່ງໃນໄລຍະຜ່ານມາຄະນະຮັບຜິດຊອບໂຄງການຂັ້ນສູນກາງ ແລະ ຫ້ອງຖິ່ນກໍ່ໄດ້ມີການປຶກສາຫາລືຜ່ານມາແລ້ວ ໂດຍສະເພາະກໍ່ແມ່ນແຜນການບໍລິຫານຄຸ້ມຄອງສິ່ງກໍ່ສ້າງຂອງໂຄງການ ແລະ ແຜນການນຳໃຊ້ທ່າເຮືອຖາວອນຂອງສະມາຊິກກຸ່ມເຮືອນ້ຳທ່ຽວ, ຜ່ານການປຶກສາຫາລື ລະຫວ່າງ ເຈົ້າຂອງໂຄງການ ແລະ ຜູ້ໃຫ້ທຶນແມ່ນໄດ້ມີການປັບປຸງແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນມີດັ່ງນີ້:

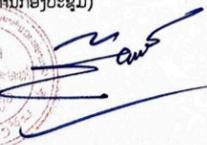
- 1) ທາງໂຄງການເຫັນດີໃຫ້ການສະໜັບສະໜູນ ຕໍ່ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນຂອງໂຄງການ ຢູ່ເຂດໂສກປາແກ້ວ-ໂສກໄທດຳ, ມີລວງຍາວໜ້ານຳປະມານ400ແມັດ ທີ່ສາມາດຈອດເຮືອໄດ້ 60-80ລຳ
 - 2) ບາງໜ້າວຽກ ຈະໄດ້ຕັດຮອນອອກເຊັ່ນ: ເສັ້ນທາງລົງທ່າເຮືອ, ທາງຢ່າງທ່ຽວລອຍ ແລະ ອື່ນໆ ເພື່ອໃຫ້ແທດເໝາະກັບຕົວເລກງົບປະມານທີ່ໂຄງການສະໜັບສະໜູນປະມານ 100,000\$ ລົງມາ.
 - 3) ອີງໃສ່ໄລຍະເວລາຂອງໂຄງການ ທາງທີມງານຊຽວຊານຈະໄດ້ອີບຮ້ອນດຳເນີນວຽກງານການຈັດຊື້-ຈັດຈ້າງ ເພື່ອໃຫ້ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນໄດ້ສຳເລັດໃນໄລຍະການມອບ-ຮັບໂຄງການ.
- ເພື່ອໃຫ້ຜູ້ເຂົ້າຮ່ວມໄດ້ປະກອບການໄດ້ແລກປ່ຽນຄຳຄິດເຫັນ, ຜ່ານການຄົ້ນຄ້ວາປຶກສາຫາລື ກອງປະຊຸມກໍ່ໄດ້ມີຄວາມເປັນເອກະພາບກັນດັ່ງນີ້.

1. ກອງປະຊຸມເຫັນດີເປັນເອກະພາບ ຕໍ່ກັບແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນຂອງໂຄງການ ຢູ່ໂສກປາແກ້ວ-ໂສກໄທດຳ ຕາມແຜນທີ່ໄດ້ມີການສຳຫລວດອອກແບບໃນໄລຍະຜ່ານມາ, ຊຶ່ງສະມາຊິກກຸ່ມເຮືອນ້ຳ ທ່ຽວທັງໝົດ ແມ່ນເຫັນດີເປັນເອກະພາບຈັດຕັ້ງປະຕິບັດຕາມແຜນບໍລິຫານ ແລະ ຄຸ້ມຄອງສິ່ງກໍ່ສ້າງຂອງໂຄງການໃນການຈອດເຮືອຄື: ຕອນກາງຄືນແມ່ນເຫັນດີເປັນເອກະພາບກັນໄປຈອດຢູ່ທ່າເຮືອຖາວອນ, ສ່ວນຕອນກາງເວັນ ຈຶ່ງສາມາດນຳເຮືອມາບໍລິການນັກທ່ອງທ່ຽວ (ຫ້າມເດັດຂາດປັບໃຫ້ມາຈອດຢູ່ບໍລິເວນຈຸດສາລາຊິມວິວ)
2. ພື້ນທີ່ຈະກໍ່ສ້າງທ່າເຮືອຖາວອນແມ່ນບໍ່ໄດ້ທັບຊ້ອນ ກັບພື້ນທີ່ເຊົ່າ-ສຳປະທານທີ່ດິນຂອງບໍລິສັດ, ຊຶ່ງໃນໄລຍະຜ່ານມາທາງຫ້ອງການກ່ຽວຂ້ອງຂອງເມືອງ ກໍ່ໄດ້ລົງສຳຫລວດວັດແທກກຳນົດເຂດໄວ້ແລ້ວຄື:
 - ຈຸດທາງລົງທ່າເຮືອຖາວອນ(ທາງລຶດໃຫຍ່) ແມ່ນກຳນົດເອົາຈຸດເຂດແດນເຊື່ອມຕໍ່ຂອງພື້ນທີ່ສຳປະທານບໍລິສັດລາຊາໂຊກ ແລະ ທ່ານ ນາງ ວົງທອງ ຈາກເສັ້ນທາງເຜີ້ງລົງໄປຫາອ່າງນ້ຳຼືມ ມີຄວາມຍາວປະມານ 800 ແມັດ, ກ້ວາງ 6 ແມັດ

- ຈຸດທາງຍ່າງລົງທ່າເຮືອຖາວອນ(ທາງຍ່າງ) ແມ່ນເລີ່ມຈາກທາງແບ່ງເຂດສາລາຕິກເບັດ(ຕາມເສັ້ນທາງຍ່າງເກົ່າ ມີຄວາມຍາວປະມານ 850ແມັດ, ກ້ວາງ 3-4 ແມັດ
 - ໃນຕໍ່ໜ້າ ຖ້າຫາກບໍລິສັດຜູ້ສຳປະທານທີ່ດິນ ມີແຜນດຳເນີນກິດຈະການເຮືອນຳທ່ຽວ ກໍ່ສາມາດມານຳໃຊ້ທ່າເຮືອຖາວອນຮ່ວມກັບສະມາຊິກກຸ່ມເຮືອນຳທ່ຽວໄດ້.
3. ມອບໃຫ້ຫ້ອງການຊັບພະຍາກອນທຳມະຊາດ ແລະ ສິ່ງແວດລ້ອມເມືອງແກ້ວອຸດົມ ໄດ້ກຳນົດແຜນຜັງລວມຂອງແຕ່ລະໂຄງການ ຢູ່ພື້ນທີ່ໂສກປາແກ້ວ-ໂສກໄທດຳ ແລະ ລວບລວມເອົາເອກະສານຕ່າງໆ ທີ່ທາງເມືອງໄດ້ອອກນິຕິກຳ ທີ່ຕິດພັນກັບພື້ນທີ່ກໍ່ສ້າງທ່າເຮືອຖາວອນ ເພື່ອເປັນຂໍ້ມູນອັນລະອຽດສະໜອງໃຫ້ກັບໂຄງການ.
 4. ທາງພະແນກ ຖວທ ແຂວງວຽງຈັນ ຈະໄດ້ປະສານສົມທົບກັບພາກສ່ວນກ່ຽວຂ້ອງ ເພື່ອດຳເນີນການປະເມີນມາດຕະຖານເຮືອນຳທ່ຽວ ແລະ ການຈັດຕັ້ງເຜີຍແຜ່ ຂໍ້ຕົກລົງຂອງທ່ານເຈົ້າແຂວງວຽງຈັນ ວ່າດ້ວຍການຄຸ້ມຄອງກິດຈະກຳການທ່ອງທ່ຽວທາງນ້ຳ ໃຫ້ກັບຜູ້ປະກອບການເຮືອນຳທ່ຽວໄດ້ຮັບຊາບ ແລະ ປະຕິບັດ ກ່ອນການເປີດບໍລິການ-ການທ່ອງທ່ຽວຄືນໃໝ່ ຢູ່ອ່າງນ້ຳງຸ່ມ.
 5. ມອບໃຫ້ທີມງານຊຽວຊານປົກປ້ອງສັງຄົມ ສືບຕໍ່ປະສານສົມທົບກັບ ຄະນະຮັບຜິດຊອບໂຄງການຂັ້ນສູນກາງ ນຳສະເໜີຕໍ່ຜູ້ໃຫ້ທຶນ ເພື່ອອະນຸມັດງົບປະມານ ໃນການກໍ່ສ້າງທ່າເຮືອຖາວອນ ຢູ່ພື້ນທີ່ເປົ້າໝາຍ ທີ່ໄດ້ມີການສຳຫລວດຮ່ວມກັນໃນໄລຍະຜ່ານມາ.
 6. ທາງຄະນະຮັບຜິດຊອບໂຄງການຂັ້ນສູນກາງ ຍັງຈະໄດ້ສືບຕໍ່ປະສານສົມທົບກັບ ຄະນະຮັບຜິດຊອບໂຄງການຂັ້ນແຂວງ ແລະ ທີມງານຊຽວຊານປົກປ້ອງສັງຄົມ ໃນການຈັດສັນ(ຈັດໂຊນ)ອາຄານຮ້ານຄ້າ ເພື່ອຄວາມເປັນລະບຽບຮຽບຮ້ອຍກ່ອນການມອບ-ຮັບໂຄງການ ໄປຕາມແຜນການບໍລິຫານຄຸ້ມຄອງ ແລະ ບຳລຸງຮັກສາຂອງໂຄງການກຳນົດໄວ້.

ກອງປະຊຸມໄດ້ປິດລົງເວລາ 16:00 ໂມງ ຂອງວັນດຽວກັນ ດັ່ງນັ້ນ, ຈຶ່ງເຮັດບົດບັນທຶກສະບັບນີ້ໄວ້ ເພື່ອເປັນບ່ອນອີງໃນການເຄື່ອນໄຫວຈັດຕັ້ງປະຕິບັດວຽກງານໃນຕໍ່ໜ້າ, 

ຫົວໜ້າພະແນກ ຖວທ ແຂວງວຽງຈັນ
(ປະທານກອງປະຊຸມ)

ຜູ້ບັນທຶກ



ສີສະຫວາດ ແສງພະຈິນ