

Social Safeguard Due Diligence Report

June 2025

Regional: Second Greater Mekong Subregion
Tourism Infrastructure for Inclusive Growth Project

TIIGP2-LAO-W02: Nam Ngum Reservoir Access
Improvements, Lao PDR

Additional works for facility improvements

CURRENCY EQUIVALENTS

Currency Units - Lao Kip (LAK)
- US Dollar (\$)
LAK 20,405 = \$1.00
(as of 30 May 2025, <https://www.bol.gov.la/referenceRate>)

WEIGHTS AND MEASURES

\$	-	United States dollar
ha	-	hectare
km	-	kilometer
m	-	meter
m ²	-	square meter

ABBREVIATIONS

ADB	Asian Development Bank
AHs	Affected Households
APs	Affected Persons
ASEAN	Association of Southeast Asian Nations
DED	Detailed Engineering Design
DICT	Department of Information, Culture and Tourism
DMS	Detailed Measurement Survey
DPWT	Department of Public Works and Transport
DRC	District Resettlement Committee
EA	Executing Agency
IPP	Indigenous Peoples Plan
Lao PDR	Lao Peoples Democratic Republic
LFNC	Lao Front for National Construction
LWU	Lao Women's Union
MICT	Ministry of Information, Culture and Tourism
MPWT	Ministry of Public Works and Transport
NPSC	National Project Steering Committee
OICT	Office of Information, Culture and Tourism
O&M	Operation and Maintenance
OWPT	Office of Public Works and Transport
PCU	Project Coordination Unit
PIU	Project Implementation Unit
PMCES	Project Management and Construction Engineering Specialists
PPSC	Provincial Project Steering Committee
PRC	Provincial Resettlement Committee
RC	Resettlement Committee
RCS	Replacement Cost Survey
RoW	Right of way
RP	Resettlement Plan
SESAH	Socio Economic Survey of Affected Households
SPS	ADB Safeguard Policy Statement (2009)
VGC	Village Grievance Committee

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I. EXECUTIVE SUMMARY

1. The Second Greater Mekong Subregion Tourism for Inclusive Growth Project is a multi-sector tourism development project being implemented in Lao PDR and Cambodia. In Lao PDR, a boat dock for tourist boats is proposed as additional works as part of the ongoing Nam Ngum Reservoir Subproject.
2. The Nam Ngum Reservoir Access Improvement Subproject is nearing completion. This Due Diligence Report (DDR) presents the results of social safeguards screening for additional works comprising (i) a boat docking facility, (ii) upgrading of internal access roads, and (iii) landscaping at the existing wastewater treatment plant. The DDR affirms that the proposed works do not trigger involuntary resettlement or indigenous peoples safeguard requirements.
3. Accordingly, the subproject is assessed as Category C for involuntary resettlement safeguard requirements and for indigenous peoples safeguard requirements.
4. Measures to mitigate temporary construction disturbance as well as health and safety risks are specified in the Environmental Management Plan (EMP) for the Nam Ngum Reservoir Subproject.
5. Monitoring for temporary construction disturbance and unanticipated impacts will be undertaken by PIU and reported to PCU in monthly reports. In case of construction impacts affecting private land, property or livelihoods this will be remedied by the contractor through prompt restoration and/or provision of compensation. Contractor temporary land use (e.g. for worker camps, storage of materials, site access, etc.) will be notified to PIU and established through written rental agreements.
6. In case of subsequent design updates, a due diligence assessment shall be conducted to check for land acquisition or resettlement impacts. If involuntary resettlement impacts are identified, a resettlement plan will be prepared.
7. Disclosure, consultation and participation has taken place during the design of the subproject and will continue during its implementation, principally through public consultation meetings to present information on the subproject, potential construction disturbance and mitigation measures, and access to the Grievance Redress Mechanism.
8. The project has an existing project Grievance Redress Mechanism (GRM), which aims to resolve complaints from project affected persons fairly, transparently, and promptly at no cost to the complainant. Complaints are first lodged with the village administration, and then escalated as needed.
9. Subproject implementation will be managed by the Project Implementation Unit (PIU) based in Keo Oudom District with supervision from the Project Coordination Union at the Ministry of Information, Culture and Tourism, and with technical assistance from the Project Implementation Consultant. Provincial and district resettlement committees are already in place, and function as grievance redress committees. Social safeguards semi-annual monitoring reports will be prepared by PCU and publicly disclosed.

II. INTRODUCTION

10. The Second Greater Mekong Subregion Tourism for Inclusive Growth Project (the project) is a multi-sector tourism development project situated in Lao PDR and Cambodia. The project has three outputs: (i) urban-rural access infrastructure and urban environmental services improved; (ii) capacity to implement ASEAN tourism standards strengthened; and (iii) institutional arrangements for tourism destination management and infrastructure O&M strengthened.

11. In Lao PDR, project activities are in Champasak, Luang Prabang and Vientiane provinces, all located along the GMS Central Economic Corridor.

12. In Vientiane Province, the Nam Ngum Reservoir Access Improvement Subproject is nearing completion. This subproject consists of three main components: (i) a new access road connecting road 10 with the Marina and upgrading the existing road from Thalath to the Marina; (ii) construction of a new Marina including retail units, recreational space, landscaping and utilities (water, wastewater, electricity, etc.).

13. This Due Diligence Report (DDR) is for additional works proposed for the Nam Ngum Reservoir Access Improvement Subproject in Vientiane Province. The purpose of this Due Diligence Report is to present the results of social safeguards screening and to affirm that the planned works do not trigger ADB involuntary resettlement or indigenous peoples safeguard requirements per the ADB Safeguard Policy Statement 2009.

III. DESCRIPTION OF PROPOSED WORKS

A. TIIGP2-LAO-W07 Boat Docking Facility

14. A new boat docking facility will be constructed to provide safe overnight mooring for 43 tourist boats that operate from the marina during the daytime.. A conceptual design for this dock was included in the updated Resettlement Plan (2023).¹ Since then, the detailed design has been finalized², with main components illustrated in Figures 1-4, and summarized as follows:

- **Mooring bollards** – Installation of 43 bollards on the shoreline for boats to tie on to.
- **Wave protection pontoons** – Installation of 2 floating pontoons with total length 180m, to provide protection from waves.
- **Mooring sinkers** - Installation of 34 concrete blocks with mooring ropes to secure the wave protection pontoons.

Figure 1 – Plan view of proposed boat dock



Legend:

- 1.1 Mooring bollards on the shoreline
- 2.1 Wave protection pontoons
- 2.2 Mooring sinkers to secure wave protection pontoons in place

¹ See Figure 9 in the Nam Ngum Reservoir Subproject Updated Resettlement Plan (2023).

² Detailed Engineering Design drawings and Bill of Quantities dated 12/02/2025.

Figure 2 – Front elevation of mooring bollard

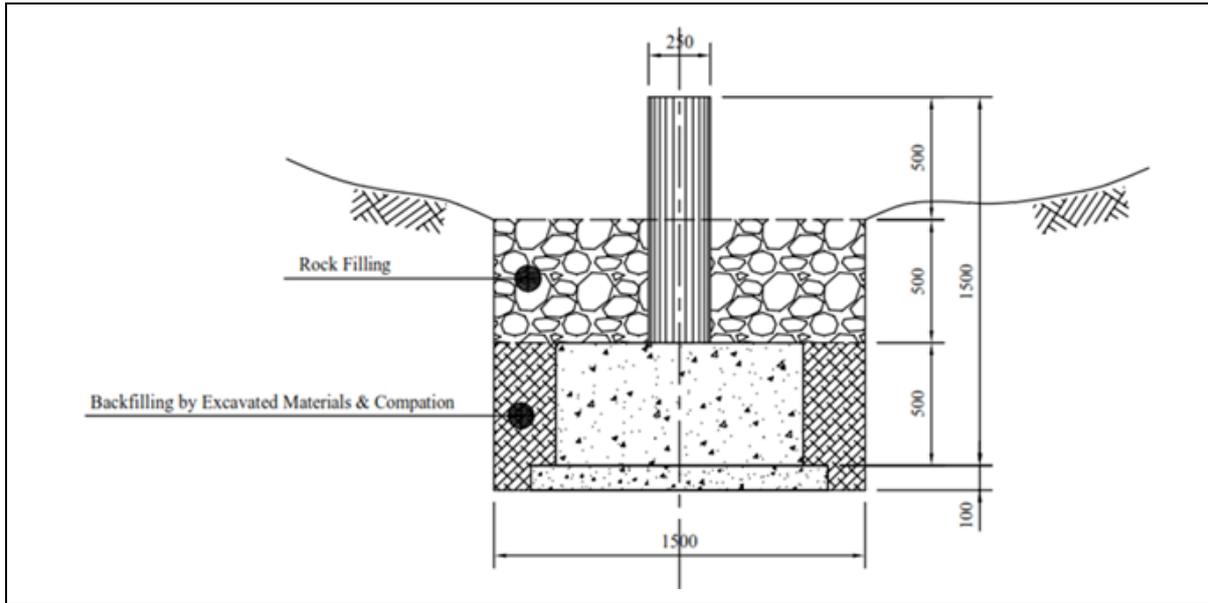


Figure 3 – Front elevation of wave protection pontoon

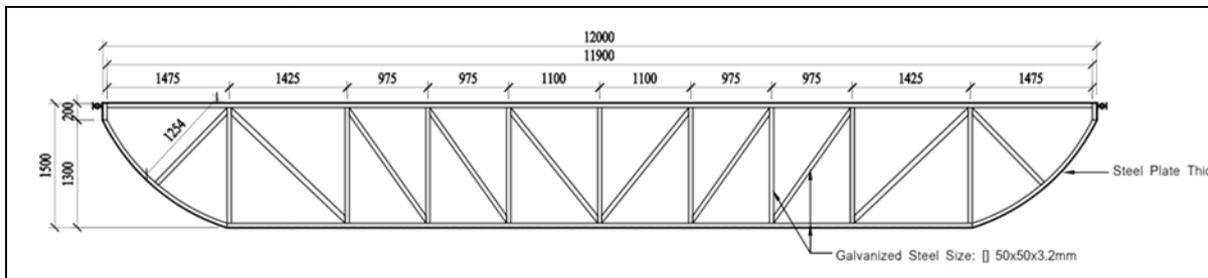


Figure 4 – Front elevation of mooring sinkers

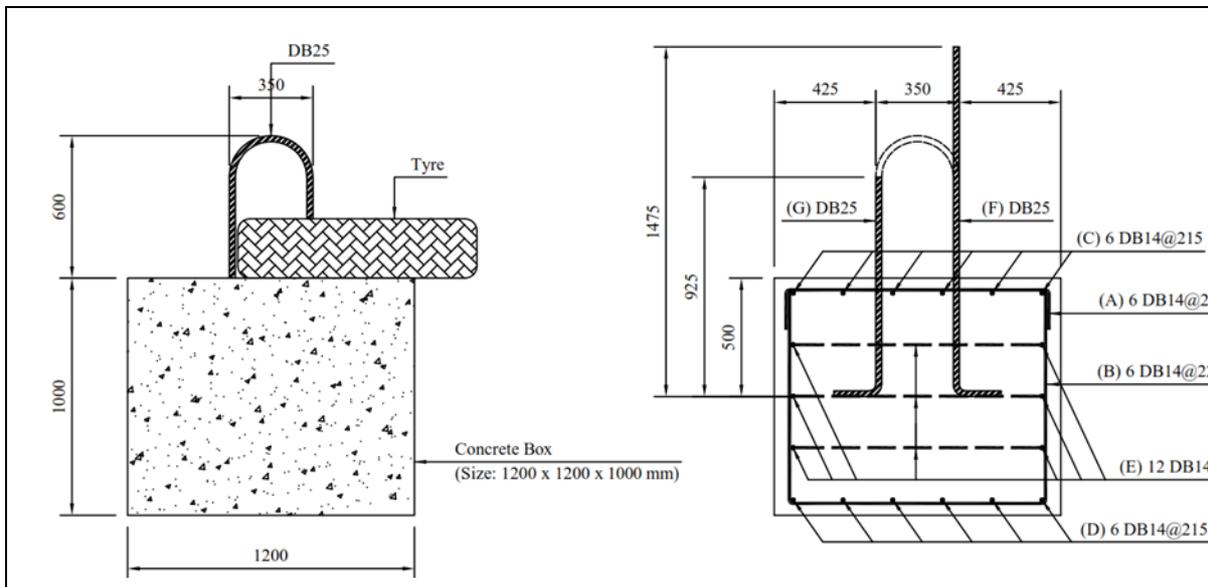


Table 1: Bill of Quantities

No.	Description	Unit	Quantity
1	Boat Docking Facilities		
1.1	Mooring Bollard for boat docking	Pieces	43.00
2	Tank for protection wave (Length = 180 m.)		
2.1	Tank for protection wave (steel plate with thickness of 4mm, dimensions 1.50m x 12.00m x 2.00m, anti-rust paint epoxy and accessories required)	Tank	15.00
2.2	Mooring sinkers (Concrete box class 20 Mpa with dimension of 1.00m x 1.00m x 1.00m) including mooring rope and installation	Set	34.00

Table 2: Material Specifications

Component	Specifications	Procurement Considerations
Mooring Bollards (Boat Docking Facility)	<ul style="list-style-type: none"> - Material: Reinforced concrete (LS 04-2010 standard) - Bollard Diameter: 250 mm - Footing Size: 1000 x 1000 x 500 mm - Lean Concrete Base: 1200 x 1200 x 100 mm - Rebar: Deformed steel bars (LS 03-2000 standard) - Surface Treatment: Marine-grade epoxy coating 	<ul style="list-style-type: none"> - Local suppliers in Vientiane City and Vientiane province can provide/supply the materials. - Ensure compliance with Lao national standards. - Verify that the epoxy coating meets marine protection standards.
Floating Wave Protection Tanks	<ul style="list-style-type: none"> - Material: Marine-grade steel (ASTM A36 or equivalent) - Plate Thickness: 4 mm - Tank Dimensions: 12000mm x 1500mm x 2000mm as shown in the design drawings - Surface Coating: Anti-rust epoxy paint (ISO 12944-6) 	<ul style="list-style-type: none"> - Local fabrication may be limited. - Consider importing raw materials or pre-fabricated tanks from Thailand/Vietnam/China. - Ensure coating is marine-grade for long-term corrosion resistance.
Mooring Sinkers (Concrete Anchor Blocks)	<ul style="list-style-type: none"> - Material: Reinforced Concrete Class 25 MPa (LS 04-2010) - Dimensions: 1200mm x 1200mm x 1000mm - Rebar: Deformed steel bars (LS 03-2000 standard) 	<ul style="list-style-type: none"> - Local concrete suppliers in Vientiane province can produce the concrete mix. - Ensure high-quality aggregates to prevent premature deterioration. - Use local Portland cement from Lao Cement or Vang Vieng Cement factories.
Mooring Ropes & Anchoring System	<ul style="list-style-type: none"> - Material: Marine-grade polyethylene rope (Ø 24mm or larger) - Breaking Strength: 100-150 kN (steel wire rope), 50-80 kN (polyethylene rope) - Corrosion Protection: Galvanized steel thimbles & shackles 	<ul style="list-style-type: none"> - Marine-grade ropes may need to be imported from Thailand/Vietnam. - Verify compliance with ISO 9554 for rope durability. - Ensure UV-resistant polyethylene ropes for longevity.

15. Component parts will be prefabricated at the contractor's workshop, transported by boat, and installed. Fabrication works will involve (i) steel cutting and welding, and (ii) reinforced concrete works. Minor earthworks (excavation, backfilling and compaction are required to install the mooring bollards on the shoreline. Rock for backfilling will be transported either by boat, or by handcart using an existing pedestrian access track. No road vehicle access to the site is required. The duration of work including fabrication and installation is estimated at 3 months.

B. TIIGP2-LAO-W08 Road Upgrades and Landscaping

16. Two existing internal unsealed roads at the marina will be upgraded to facilitate site access and traffic flow. The civil works will include installation of reinforced concrete pavement, road drains and embankment slope protection. The location and road layout is shown in Figure 5 and Figure 6 below.

17. Also in this work package, landscaping works will be undertaken around the existing water treatment plant. The civil works will comprise stone masonry walls, embankment slope protection and fencing works. The location and layout of works is shown in Figure 7 and Figure 8 below.

Figure 5 –View of roads to be upgraded



Figure 6 –Layout drawing of road upgrades

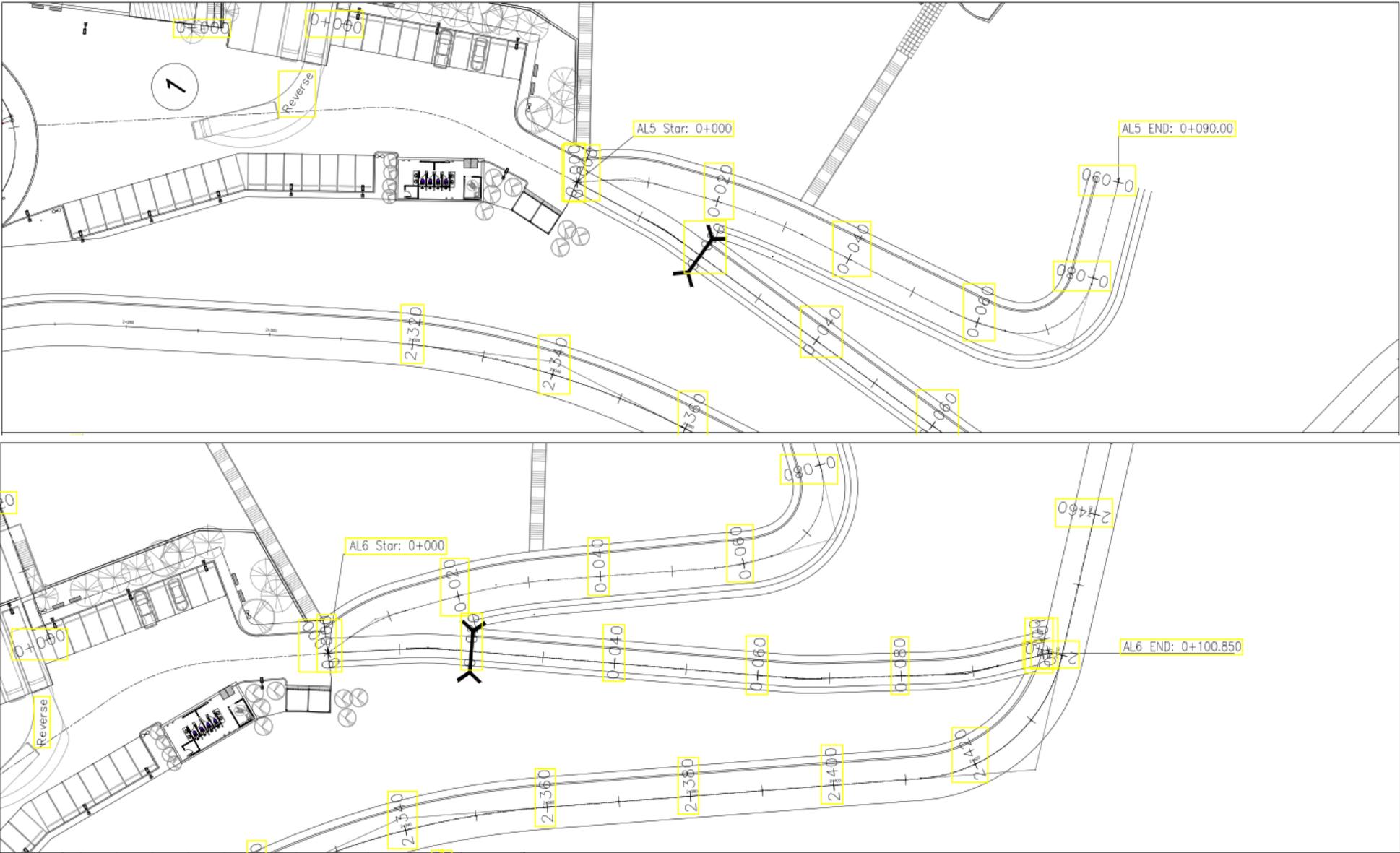


Figure 7 – Map showing location of water treatment plant

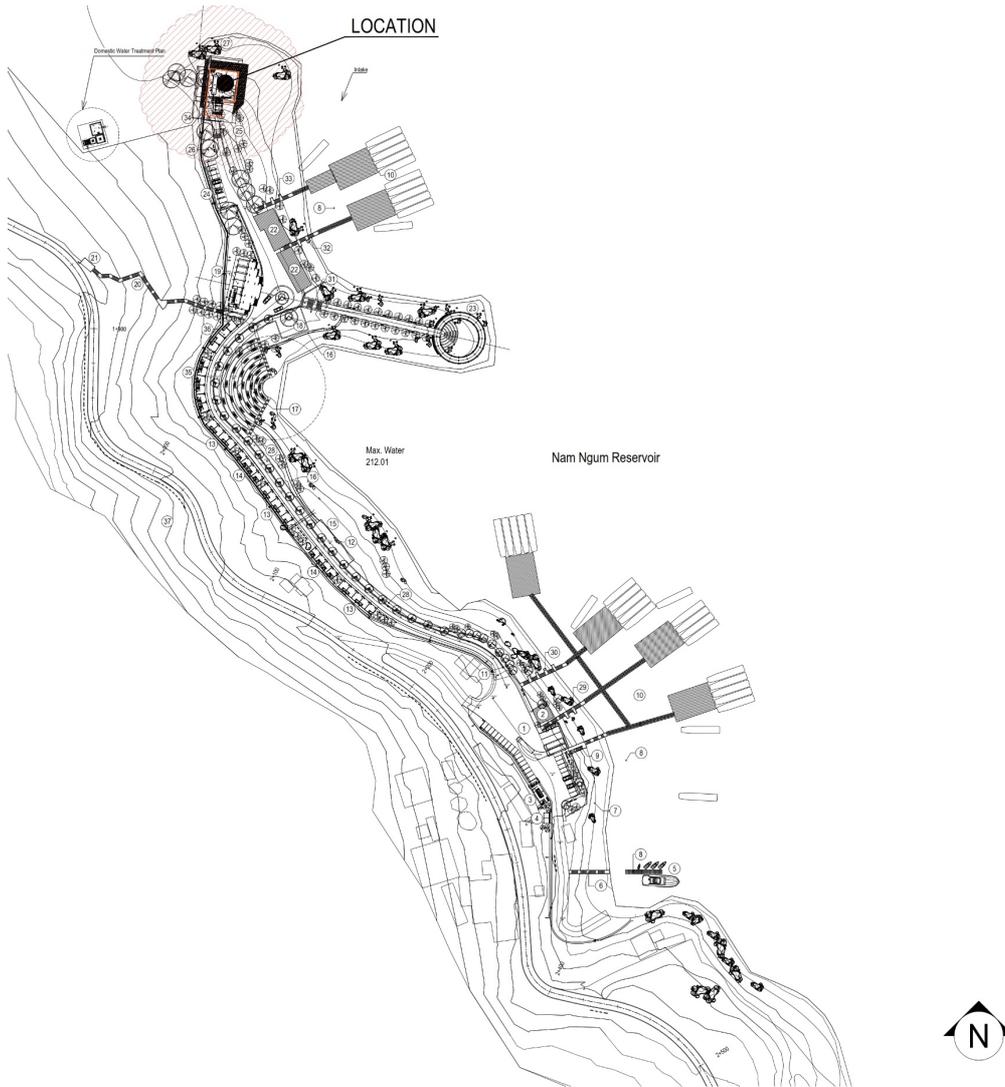
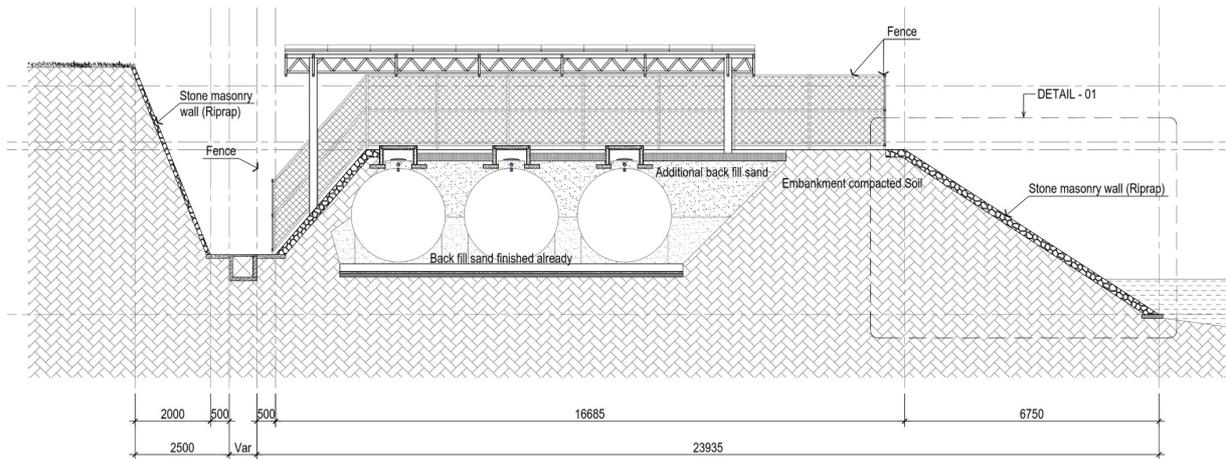


Figure 8 – Cross-section drawing of works at the water treatment plant



IV. DUE DILIGENCE FINDINGS

18. Due diligence was carried out in December 2024 by the project's Social Safeguard Specialists in consultation with PCU, PIU and PMCES. The process involved:

- Review of the final draft detailed engineering design report and layout drawings.
- A site inspection of the location proposed.
- A stakeholder consultation to disclose information about the subproject and receive feedback.
- Screening for involuntary resettlement impacts with reference to the ADB Safeguards Policy Statement.

19. The subproject is assessed as Category C for involuntary resettlement safeguard requirements. The subproject is also assessed as Category C for indigenous peoples safeguard requirements as no ethnic minority populations will be affected by the works. The categorization is derived from the findings of due diligence, which affirm that the planned works will not cause any involuntary resettlement impacts. The screening checklists for each work package are enclosed as Appendix A.

A. TIIGP2-LAO-W07 Boat Docking Facility

20. The boat dock is a floating facility on the Nam Ngum 1 reservoir, for which permission was formally granted by the Keo Oudom District Governor in March 2022 (see Appendix B1). Boat mooring bollards will be installed on the shoreline which is public land³, and is unused (contains no private structures, is not used for agriculture or other livelihoods activities). Access to the dock during construction and operations will be mainly by boat, as well as via an existing public footpath (see map in Appendix B1).

B. TIIGP2-LAO-W08 Road Upgrades and Landscaping

The road upgrades and landscaping works around the wastewater treatment plant will not require land acquisition, as the location is inside the boundary of the Nam Ngum 1 Marina which is state land. There are no houses or other structures in the vicinity. Civil works for the road upgrades will not adversely affect the operation of the nearby public ferry port or the nearby hotel (Long Ngum Resort) as there is a separate access road serving these two sites. The wastewater treatment plant is situated at the end of the marina site, accessible via a paved road.

21. Risks of **construction disturbance** as well as public **health and safety** will be mitigated and managed through measures contained in the Environmental Management Plan (EMP) and will be incorporated in each contractor's EMP (CEMP). They include the following:

- Measures to minimize noise, dust and other construction disturbance to ensure commercial activities at the marina are not adversely affected.
- Measures to ensure worker and community health and safety during construction, including during the transportation of materials to and from site..

22. Temporary construction disturbance will be monitored by PIU through regular monitoring visits to work sites together with a representative of the District Resettlement Committee and the head of the boat association. The monitoring will aim to pre-empt construction disturbance before it occurs, and rectify disturbance that does occur.

23. **Contractor's temporary land use requirements** (e.g. for worker camps, storage of materials, site access, etc.) will be notified to PIU prior to work commencing. The contractor

³ Decree on Wetlands (November 2023), Article 15, defines land 15m from the waterline of lakes/reservoirs as state reserve land.

will make a formal rental agreement for all temporary use and will provide a copy to PIU. At the end of the agreed period of use, the contractor will restore the land to its original condition (or as stipulated in the rental agreement) and will sign obtain written confirmation from the landowner that restoration has been completed satisfactorily. PIU will keep a record of contractor land rental agreements, proof of rental payment, and evidence of restoration and handover upon completion of works.

24. **Unanticipated/unintended construction impacts** on private land, property or livelihoods will be remedied by the contractor through prompt restoration and/or provision of compensation or other entitlements as laid out in the subproject's updated Resettlement Plan (2023).

25. In case of **subproject design modifications**, social safeguards screening will be carried out. Either this due diligence report will be updated or, if involuntary resettlement impacts are identified, a resettlement plan will be prepared.

V. DISCLOSURE, CONSULTATION AND PARTICIPATION

26. In the context of the proposed works, the objectives of disclosure, consultation and participation are:

- (i) To fully inform stakeholders about the objectives and scope of proposed works, social safeguards screening results, anticipated risks and mitigation measures during construction, and plans for operation and maintenance after completion.
- (ii) To solicit feedback from stakeholders to enhance the design and implementation of civil works; and
- (iii) To inform APs how to access the Grievance Redress Mechanism

27. Key stakeholders for the purpose of disclosure, consultation and participation are:

- (i) Boat owners (final users of the boat docking facility).
- (ii) Business owners operating shops and restaurants at the Marina.
- (iii) The Marina site management company
- (iv) Owners of nearby state land concessions.
- (v) Project Implementation Unit
- (vi) District Resettlement Committee
- (vii) Local government officers at village, district and province level who are involved in the project.

A. Public Consultations during Preparation of this Due Diligence Report

28. Detailed consultation have been held with members of the tour boat association on the design and layout of the boat dock facility, particularly the location of boat mooring bollards and the location of wave break pontoons. On 30 August 2024 a stakeholder meeting was held to review and confirm the final design for the boat dock. During these stakeholder consultations, information was presented on the proposed design, scope of works, the safeguards screening, and the Grievance Redress Mechanism. A summary of consultations is provided in Table 3 and consultation records are in Appendix B.

Table 3: Consultations during Design Phase

Date	Location	Participants from (total / female)	Summary of main topics and discussion
30/8/2024	Nam Ngum Marina	Vice District Governor Provincial Tourism Dept, District Tourism Dept, PCU safeguard consultants Boat association members Nearby concession holders Total: 18 (female 4)	<ul style="list-style-type: none"> • Presentation of proposed design. • Agreed reduction in scope to meet budget ceiling (exclude access road). • Agreed to expedite civil works to provide permanent mooring. • Agreed that tourist boats will not be permitted to moor at the tourist marina as this would block the view for visitors. • Confirmed there will not be any impact on existing land rights or land use. • Confirmed the public access track has a right of way 6m wide. • Agreed that future (new) tourist boat operators will also have the right to use the dock.

Date	Location	Participants from (total / female)	Summary of main topics and discussion
23/12/2024	Nam Ngum Marina	PIU District Tourism Dept, PCU safeguard consultants Boat association members Total: 10 (female 3)	<ul style="list-style-type: none"> Presentation of detailed engineering design. Design agreed and accepted by members of the Boat Association.

B. Planned Consultations during Implementation of Additional Works

29. Consultation and disclosure activities with project stakeholders will continue during subproject implementation, including prior to commencing civil works and periodically during the construction phase, as shown in the table below.

Table 4: Planned consultations during Implementation

Timing	Location	Participants	Consultation topics
Prior to works starting	Nam Ngum Marina	Boat Association (users of the boat dock), business operators at the marina, PIU, PIC, DRC, Civil work contractor	Construction schedule and methods. Due Diligence Findings. EMP mitigation measures. Grievance Redress Mechanism.
Quarterly during civil works	Nam Ngum Marina	Boat Association (users of the boat dock), business operators at the marina, PIU, PIC, DRC Civil work contractor	Presentation and discussion on: -Construction progress/issues - EMP mitigation measures -Unanticipated impacts -Grievances
Upon completing civil works	Nam Ngum Marina	Boat Association (users of the boat dock), business operators at the marina, PIU, PIC, DRC Civil work contractor	Construction defects (if any) and agreed corrective measures. User-feedback on completed works. Grievances outstanding (if any)

C. Disclosure of information

30. Information about the subproject has been provided to stakeholders during the preparation (Table 3, above), including about the design of the boat dock and scope of works, the results of social safeguards screening, identified risks and mitigation measures, safeguard principles and policies, and access to the Grievance Redress Mechanism.

31. Disclosure of information will continue as part of consultation activities during subproject implementation (see Table 4, above)

32. This Due Diligence Report will be publicly disclosed on the ADB website, along with 6-monthly social safeguards monitoring reports covering the entire project.

VI. GRIEVANCE REDRESS MECHANISM

33. Anyone affected by the project is entitled to lodge grievances⁴. The Grievance Redress Mechanism (GRM) already place for the Nam Ngum Access Road Improvement Subproject will also serve for the additional works within the scope of this due diligence report. The GRM is described in detail in the subproject's updated Resettlement Plan (2023) and for brevity is not repeated here in full. In summary, the GRM lays out a procedure for receiving, registering, investigation and assessing grievances, and for resolving them through mediation and negotiation to the satisfaction of the applicant and in accordance with the safeguard principles and policies defined in the updated Resettlement Plan (2023) and ADB SPS (2009). The GRM has 4 levels of escalation for cases that are complex and in case the applicant does not accept the resolution proposed. The GRM aims to provide accessible, swift and fair remedy; however, applicants are also entitled to seek judicial remedy through the courts if preferred. Applicants are also entitled to contact the ADB directly, in accordance with the ADB Accountability Mechanism. Information about the Grievance Redress Mechanism (GRM) has been communicated during stakeholder consultations and communication will continue during the period of civil works.

VII. IMPLEMENTATION ARRANGEMENTS

A. Implementation Schedule

34. Implementation of measures in this DDR pertaining to public consultation, monitoring and reporting will be aligned with the construction schedule, as shown in Table 5 below.

Table 5: Schedule of DDR Implementation

Milestone/Activity	Schedule
ADB review, concurrence and disclosure of this DDR	June 2025
Procurement process and bid award	June-July 2025
Pre-construction public consultation meeting	August 2025
Start of civil works	August 2025
Social safeguards monitoring and reporting during construction	August-December 2025

B. Roles and Responsibilities

35. The additional works involved in establishing the boat dock facility will be implemented using existing subproject institutional arrangements. This includes various levels of government including the Ministry of Information Culture and Tourism (MICT) and its line agencies at province and district level, Resettlement Committees at provincial and district level, village authorities, PIU, PCU and Project Implementation Consultants. The institutional arrangements are summarized in the paragraphs that follow. Additional details are provided in the Project Administration Manual (PAM). Training will be provided to ensure personnel involved implementation have capacity to efficiently perform their functions.

1. Executing Agency

36. The Ministry of Information, Culture and Tourism (MICT) is the Executing Agency (EA) for the Project. As EA, its responsibilities include: (i) overall management of the project; (ii) establishment of the National Project Steering Committee (NSPC) headed by the Vice-

⁴ Grievance is considered an issue, concern, problem, or claim (perceived or real) that an individual or group wants the Project to address and resolve

Minister with representatives from related Ministries, (iii) establishment of a Project Coordination Unit (PCU) at MICT; (iv) recruitment of project implementation consultants; (v) approval of the project's annual implementation plans and other related project plans and budget; and, (vi) reporting to ADB and Lao Government on project status.

2. Project Coordination Unit

37. MICT has established a Project Coordination Unit (PCU) responsible for: (i) day-to-day coordination of all aspects of the project including social safeguards; (ii) acting as secretariat to the NSPC by preparing reports and coordinating NSPC meetings, the annual project review and annual planning; (iii) prepare project progress reports on behalf of the EA and submitting these to ADB and Lao Government; (iv) coordinating with the Project Implementation Unit (PIU) in Vientiane Province; and (v) designating staff as safeguards focal points to supervise and monitor the progress of the safeguards plans.

3. Project Implementation Unit

38. In Vientiane Province, the Department of Information Culture and Tourism (DICT) has established a Project Implementation Unit (PIU) responsible coordinating day-to-day activities related to Project Output 2 and 3 as well as the infrastructure works for the Nam Ngum Reservoir Access Improvement Subproject. PIU receives technical support from the Department of Public Works and Transport (DPWT), Vientiane Province. PIU has designated a coordinator/focal person for safeguards and gender implementation and monitoring. DICT prepares and submits progress reports to PCU, coordinates with district level stakeholders including the district resettlement committees, and acts as technical secretariat to the Provincial Project Steering Committee (PPSC).

4. Resettlement Committees

39. A Provincial Resettlement Committee (PRC) has been established at Vientiane Province with the following functions: (i) participate in consultation meetings with APs, (ii) prepare and endorse compensation unit rates; (iii) review and confirm impacts based on the detailed measurement survey; (iv) participate in consultations with APs to agree on compensation; (v) supervise compensation payments to APs and prepare a disbursement report for submission to the Provincial Department of Finance; (vi) monitor and report on all social safeguard activities; (vii) act as grievance officers to deliberate and resolve resettlement-related complaints from APs, and (viii) advise PCU on the issuance of a notice-to-proceed with civil works when compensation and relocation has been completed so that site clearance can be undertaken.

40. The Keo Oudom District Resettlement Committee (DRC) works under the supervision of the PRC. The District Resettlement Committee (DRC) comprises officials from relevant district departments, as well as representatives from the Lao Women's Union (LWU) and Lao Front for National Construction (LFNC) and is headed by the Deputy District Governor.

41. Village Resettlement Committees support the work of the PIU and DRC. They are composed of the Village Chief, representatives of LWU and LFNC, and village elders. The VRC is responsible for the following:

- Coordinate public consultations with villagers.
- Participate in resettlement planning activities including DMS, SESA and replacement cost survey.
- Mobilize APs to actively participate in the resettlement planning process.
- Assist in identifying replacement land for physically displaced persons.
- Co-sign compensation agreements.
- Receive, record, investigate, mediate and resolve grievances.

42. In the context of the additional works proposed to install a boat docking facility, considering the absence of resettlement impacts, the main role of resettlement committees will be to:

- Participate in public consultations with members of the boat association and local community stakeholders
- Receive, record, investigate, mediate and resolve grievances.

5. Project Implementation Consultants

43. The PCU is supported by a Project Management and Civil Engineer Support (PMCES) Consultant comprising a team of technical specialists. PCU also has several individual consultants directly managed by PCU.

44. Two Social Safeguards Specialists (one national, one international) contracted directly to PCU have responsibility for:

- Safeguard screening and due diligence reports (for activities not triggering resettlement or indigenous peoples requirements).
- Preparing and updating resettlement plans for subprojects that trigger resettlement or indigenous peoples requirements.
- Devising strategies and materials for public disclosure, consultation and participation.
- Preparing survey methods and tools for resettlement surveys (DMS, SESA, RPC).
- Supporting the PCU Institutional Strengthening Specialist and Tourism Development Specialist in designing training/capacity building for affected households and in particular vulnerable groups (Output 2 & 3).
- Capacity building and advice to PCU, PIU and resettlement committees on social safeguard obligations with reference to ADB SPS, including compensation, indigenous peoples safeguards, public consultation, participation and disclosure, and grievance redress.
- Ensuring the grievance redress mechanism is functioning well.
- Establishing internal monitoring procedures for PCU and PIU social safeguard focal points, and providing training.

VIII. Monitoring and Reporting

C. Internal Monitoring

45. The existing monitoring arrangements for the project and the Nam Ngum Reservoir Access Improvement Subproject will continue during implementation of works covered by this Due Diligence Report. These are described in detail in the updated Resettlement Plan (2023).

46. The EA has overall responsible for internal monitoring, which is conducted by PCU and PIU in coordination with designated provincial and district safeguard focal points and with technical support from the International and National Social Safeguards Specialists.

47. PCU prepares monthly and quarterly project implementation reports, as well as a semi-annual safeguard monitoring report which highlights: i) accomplishments in relation to targets; ii) resettlement implementation issues and (iii) actions taken to resolve issues and/or issues for EA decision. Semi-annual safeguard monitoring reports are submitted to ADB for review and publicly disclosed on the ADB website.

Table 6: Internal Monitoring Framework

Monitoring	Objectives	Time Frame	Responsible Project Units
Quarterly monitoring and reporting	Assess compliance with social safeguard plans and mitigation measures, identify issues and concerns, recommend corrective actions.	PIU submits it to PCU by 2nd weeks of the end of the quarter, and PCU sends as part of Quarterly report to ADB by 4 th week of the end of the quarter.	PIUs at District level and PCU at national/ project-wide level, supported by project implementation consultants
Semi-annual monitoring missions and safeguards monitoring reports	Assess compliance with RP. Provide guidance to resolve any implementation or compliance issues.	Two missions and safeguards monitoring reports per year.	ADB, PCU and PIUs supported by project implementation consultants
Project completion report	Assess compliance with RP. Assess achievements and lessons from RP implementation and CCRs	Within 6 months of physical completion of the project	PCU and PIUs supported by project implementation consultants

Source: Compiled by MICT, updated in December 2021

D. External Monitoring

48. External monitoring of the social safeguard implementation regarding the Nam Ngum Access Improvement subproject is carried out by an External Monitor (EM) who conducts periodic field visits and submits monitoring reports to PCU and ADB. The EM role is required due to the subproject being Category A for IR. As the additional works involved in establishing a boat docking facility are assessed as Category C, the focus of external monitoring will be to validate the absence of resettlement impacts, and evaluate the adequacy of stakeholder consultation and grievance redress.

IX. APPENDICES

A. Social Safeguards Screening Checklists

1. Additional work package TIIGP2-LAO-W07

Brief Description of Works:	Boat docking facility comprising wave break pontoons, mooring sinkers and mooring bollards.
Design Stage:	Detailed Design

Screening Questions	Yes	No	Remarks
1. Will there be land acquisition?		√	The dock will be located on the reservoir. The only land-based component is mooring bollards on the shoreline which is state land.
2. Is the site for land acquisition known?	N/A		No land acquisition is anticipated
3. Is the ownership status and current usage of land to be acquired known?	N/A		No land acquisition is anticipated
4. Will easement be utilized within an existing Right of Way (RoW)?		√	Access during construction and operations will primarily use boats. There is an existing pedestrian path to the shoreline which may also be used.
5. Will there be loss of shelter and residential land?		√	No houses or shelters on the shoreline where mooring bollards will be installed.
6. Will there be loss of agricultural and other productive assets?		√	No agricultural or other productive activities on the shoreline where mooring bollards will be installed. This area of the reservoir is not used by local fishers.
7. Will there be losses of crops, trees, and fixed assets?		√	There are no crops, trees or other private assets on the shoreline where mooring bollards will be installed.
8. Will there be loss of businesses or enterprises?		√	None anticipated. Conversely, the works will support local boat businesses.
9. Will there be loss of income sources and means of livelihoods?		√	None anticipated..
10. Will people lose access to natural resources, communal facilities, and services?		√	No natural resources, communal facilities or services will be affected.
11. If land use is changed, will it have an adverse impact on social and economic activities?		√	No land use change.
12. Will access to land and resources owned communally or by the state be restricted?		√	No restrictions due to the proposed works.
13. Are there socio-cultural groups present in or use the project area who may be considered as "tribes", ethnic or national minorities, or indigenous communities in the project area?		√	Beneficiaries are owners of four boats operating on the marina, none of whom are indigenous peoples.

Involuntary Resettlement Safeguards Category [√]			Safeguards Document Required
A		Significant involuntary resettlement impacts	Due Diligence Report
B		Involuntary resettlement impacts that are not significant.	
C	√	No involuntary resettlement impacts	

Indigenous Peoples Safeguards Category [√]			Safeguards Document Required
A		Significant impacts on indigenous peoples	Due Diligence Report.
B		Limited adverse impacts on indigenous peoples	
B		Beneficial only impact on indigenous peoples	
C	√	No impacts on indigenous peoples.	

2. Additional work package TIIGP2-LAO-W08

Brief Description of Works:	Improvement of internal access roads at Nam Ngum 1 Reservoir Recreational Area and landscaping works at the wastewater treatment plant.
Design Stage:	Detailed Engineering Design

Screening Questions	Yes	No	Remarks
1. Will there be land acquisition?		√	No land acquisition is required as all civil works are within the site of the Nam Ngum 1 Marina, which is public (government) land.
2. Is the site for land acquisition known?	N/A		No land acquisition is anticipated
3. Is the ownership status and current usage of land to be acquired known?	N/A		No land acquisition is anticipated
4. Will easement be utilized within an existing Right of Way (RoW)?	√		Two existing unsealed roads will be improved (AL-05 and AL-06).
5. Will there be loss of shelter and residential land?		√	No houses, shelters or other structures will be affected by the works.
6. Will there be loss of agricultural and other productive assets?		√	No agricultural or other productive assets will be affected. This area of the reservoir is not used by local fishers..
7. Will there be losses of crops, trees, and fixed assets?		√	There are no crops, trees or other fixed assets along the road alignment or at the water treatment plant.
8. Will there be loss of businesses or enterprises?		√	None anticipated. Customer access to the nearby Long Ngum Resort will not be obstructed by the proposed works. Temporary construction disturbance (e.g. noise, vibration, dust) will be managed through measures in the EMP.
9. Will there be loss of income sources and means of livelihoods?		√	None anticipated.
10. Will people lose access to natural resources, communal facilities, and services?		√	No natural resources, communal facilities or services will be affected. The public ferry port, which is operating from a nearby jetty outside the project footprint, will not be affected.
11. If land use is changed, will it have an adverse impact on social and economic activities?		√	The works will not require land use change.
12. Will access to land and resources owned communally or by the state be restricted?		√	The works will not entail any access restrictions; on the contrary the road upgrade will improve access to the marina and associated tourist facilities.
13. Are there socio-cultural groups present in or use the project area who may be considered as "tribes", ethnic or national minorities, or indigenous communities in the project area?		√	There are no villages, communities or residential properties in the immediate vicinity of the proposed works. The beneficiaries of the works will be visitors to the marina and small businesses based there.

Involuntary Resettlement Safeguards Category			Safeguards Document Required
A		Significant involuntary resettlement impacts	Due Diligence Report
B		Involuntary resettlement impacts that are not significant.	
C	√	No involuntary resettlement impacts	

Indigenous Peoples Safeguards Category			Safeguards Document Required
A		Significant impacts on indigenous peoples	Due Diligence Report
B		Limited adverse impacts on indigenous peoples	
B		Beneficial only impact on indigenous peoples	
C	√	No impacts on indigenous peoples.	

B. Public Consultation Records

1. Agreement for location of the Dock, March 2022



ສາທາລະນະລັດ ປະຊາທິປະໄຕ ປະຊາຊົນລາວ
ສັນຕິພາບ ເອກະລາດ ປະຊາທິປະໄຕ ເອກະພາບ ວັດທະນະຖາວອນ

ແຂວງວຽງຈັນ
ເມືອງແກ້ວອຸດົມ

ເລກທີ. 12.1./ຈມ.ກດ
ລົງວັນທີ 14/03/2022

ຂໍ້ຕົກລົງ

ວ່າດ້ວຍ ການກຳນົດທາງລົງທຳເຮືອ ແລະ ສະຖານທີ່ຈອດເຮືອຖາວອນຂອງລັດຖຸມຄອງ
ຢູ່ໜ້າເຂື່ອນນ້ຳງື່ມ 1 (ໂສກປາແກ້ວ) ບ້ານແສງສະຫວ່າງ ເມືອງແກ້ວອຸດົມ

- ອີງຕາມ ກົດໝາຍປົກຄອງທ້ອງຖິ່ນ ສະບັບເລກທີ 68/ສພຊ, ລົງວັນທີ 14 ທັນວາ 2015.
- ອີງຕາມ ກົດໝາຍວ່າດ້ວຍທີ່ດິນ ສະບັບເລກທີ 70/ສພຊ, ລົງວັນທີ 21 ມິຖຸນາ 2019.
- ອີງຕາມ ໜັງສືສະເໜີຂອງກຸ່ມເຮືອນຳທ່ຽວອ່າງນ້ຳງື່ມ 1 ສະບັບເລກທີ 01/ກຮທ, ລົງວັນທີ 16 ມັງກອນ 2022.

ເຈົ້າເມືອງຕົກລົງ:

ມາດຕາ 1 ຕົກລົງກຳນົດຂອບເຂດທາງລົງທຳເຮືອ ເຊິ່ງມີຄວາມກວ້າງ 6 ແມັດ, ຍາວ 186,65 ແມັດ, ລວມເນື້ອທີ່ທັງໝົດ 1,070 ຕາແມັດ ຫຼື ເທົ່າກັບ 0,107 ເຮັກຕາ ແລະ ທາງຢາງລົງທຳເຮືອລະຫວ່າງເຂດແດນທ່ານ ຫຸ້ມພັນ ສະເໜີຂໍສຳປະທານ ແລະ ບໍລິສັດລາຊາໂຊກສະເໜີຂໍສຳປະທານເບື້ອງລະ 2 ແມັດ ເຊິ່ງລວມຄວາມກວ້າງ 4 ແມັດ, ຍາວ 112,42 ແມັດ, ລວມເນື້ອທີ່ທັງໝົດ 448 ຕາແມັດ ເທົ່າກັບ 0.045 ເຮັກຕາ.

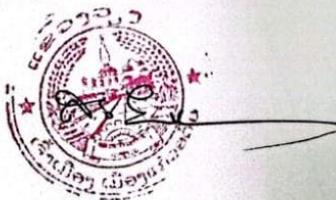
ມາດຕາ 2 ກຳນົດຂອບເຂດທ່າຈອດເຮືອທີ່ລັດຖຸມຄອງ ໂດຍມອບໃຫ້ກຸ່ມເຮືອນຳທ່ຽວເປັນຜູ້ນຳໃຊ້ ເພື່ອເປັນບ່ອນຈອດເຮືອຖາວອນຢູ່ໜ້າເຂື່ອນນ້ຳງື່ມ 1 ເຊິ່ງມີຄວາມກວ້າງ 50 ແມັດ, ຍາວ 290 ແມັດ ເບື້ອງໜ້າຕິດກັບຕອນດິນ ແລະ ເບື້ອງຫຼັງຕິດກັບນ້ຳງື່ມມີຄວາມຍາວ 300 ແມັດ, ລວມເນື້ອທີ່ທັງໝົດ 14,504 ຕາແມັດ, ເທົ່າກັບ 1,450 ເຮັກຕາ ໂດຍມອບໃຫ້ໂຄງການ ADB ເປັນຜູ້ພັດທະນາ, ຫຼັງຈາກພັດທະນາສຳເລັດແລ້ວແມ່ນ ມອບຄືນໃຫ້ກຸ່ມເຮືອນຳທ່ຽວເປັນຜູ້ນຳໃຊ້ຕາມລະບຽບການ.

ມາດຕາ 3 ສຳລັບພື້ນທີ່ດັ່ງກ່າວ ຫ້າມເອົາໄປຈັດສັນໃຫ້ພະນັກງານຖຸມຄອງນຳໃຊ້, ແລກປ່ຽນ, ຄ້າປະກັນ, ໂອນສິດ ຫຼື ຊື້-ຂາຍ ໃຫ້ບຸກຄົນ, ນິຕິບຸກຄົນ, ການຈັດຕັ້ງຂອງລັດ ແລະ ຫ້າມນຳໃຊ້ໄປໃນທາງທີ່ຜິດຕໍ່ກົດໝາຍ.

ມາດຕາ 4 ໃຫ້ການຈັດຕັ້ງຂອງລັດ, ພາກສ່ວນທີ່ກ່ຽວຂ້ອງຈົ່ງຮັບຮູ້ ແລະ ຈັດຕັ້ງປະຕິບັດຕາມຂໍ້ຕົກລົງສະບັບນີ້ຢ່າງເຂັ້ມງວດ.

ມາດຕາ 5 ຂໍ້ຕົກລົງສະບັບນີ້ ມີຜົນສັກສິດ ແລະ ນຳໃຊ້ໄດ້ ນັບແຕ່ວັນລົງລາຍເຊັນເປັນຕົ້ນໄປ.

ຜູ້ຮອງ ເຈົ້າເມືອງ



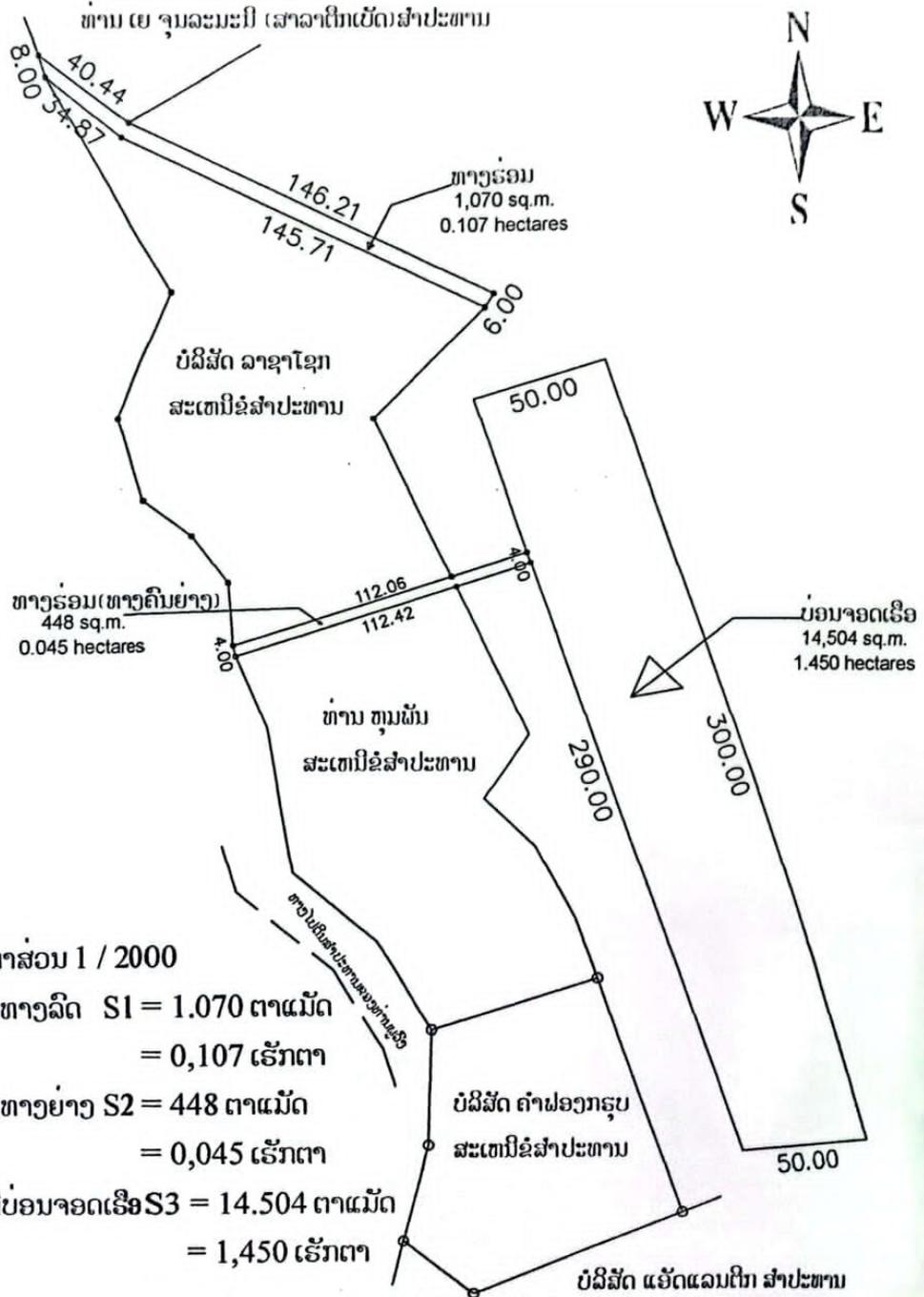
ສິງຄຳ ມະນີວິນ

Shot on Y12
Vivo AI camera
WIDE

ສາທາລະນະລັດ ປະຊາທິປະໄຕ ປະຊາຊົນລາວ
 ສັນຕິພາບ ເອກະລາດ ປະຊາທິປະໄຕ ເອກະພາບ ວັດທະນາຖາວອນ

ແຜນວາດດິນລັດ

ປະທານກຸ່ມເຮືອນນໍ້າທ່ຽວໜ້າເຂື່ອນນ້ຳຢູ່ນ1 ສະເໜີຂໍເສັ້ນທາງລົງໄປຫາເຮືອ ແລະ ຂໍບ່ອນຈອດເຮືອຖາວອນ
 ຢູ່ເຂດໂສກຄຸ້ມໂຫດໍາ ບ້ານ ແສງສະຫວ່າງ ເມືອງ ແກ້ວອຸດົມ ແຂວງ ອຽງຈັນ.



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2. Stakeholder Consultation on Final Design, August 2024



ສາທາລະນະລັດ ປະຊາທິປະໄຕ ປະຊາຊົນລາວ
ສັນຕິພາບ ເອກະລາດ ປະຊາທິປະໄຕ ເອກະພາບ ວັດທະນະຖາວອນ

ບົດບັນທຶກ

ກອງປະຊຸມປຶກສາຫາລື ກ່ຽວກັບ ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນ ຂອງໂຄງການ(ADB)
ຢູ່ອ່າງນ້ຳຼືມ ເມືອງແກ້ວອຸດົມ ແຂວງວຽງຈັນ

ໃນຕອນແລງເວລາ 13:30 ໂມງ ຂອງວັນທີ 30/8/2024 ທີ່ຫ້ອງປະຊຸມ ຫ້ອງການຍຸຕິທຳເມືອງແກ້ວອຸດົມ ໄດ້ຈັດກອງປະຊຸມປຶກສາຫາລື ກ່ຽວກັບ ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນ ຂອງໂຄງການ(ADB) ຢູ່ອ່າງນ້ຳຼືມ ເມືອງແກ້ວອຸດົມ, ໂດຍໃຫ້ກຽດເປັນປະທານຮ່ວມຂອງ ທ່ານ ສິສະຫວາດ ແສງພະຈັນ ຫົວຫົວໜ້າພະແນກຖວທ ແຂວງວຽງຈັນ ແລະ ທ່ານ ສິງຄຳ ມະນີວັນ ຮອງເຈົ້າເມືອງແກ້ວອຸດົມ, ມີທີມງານຊຽວຊານປົກປ້ອງສັງຄົມ, ຄະນະໄກ່ເກ່ຍ, ສະມາຊິກກຸ່ມເຮືອນຳທ່ຽວອ່າງນ້ຳຼືມ ແລະ ຫົວໜ່ວຍທຸລະກິດເຊົ່າ-ສໍາປະທານທີ່ດິນ ເຂດໄກ້ຄຽງກັບພື້ນທີ່ຈະກໍ່ສ້າງທ່າເຮືອຖາວອນຂອງໂຄງການຢູ່ເຂດໂສກປາແກ້ວ-ໂສກໄທດຳ ເຂົ້າຮ່ວມ ດັ່ງມີລາຍຊື່ຕັດຕິດມາພ້ອມນີ້.

ໃນກອງປະຊຸມ ກໍ່ໄດ້ຮັບຟັງການລາຍງານສະພາບລວມ ແລະ ຄວາມຈຳເປັນຂອງການທ່າເຮືອຖາວອນຂອງໂຄງການ, ຊຶ່ງໃນໄລຍະຜ່ານມາຄະນະຮັບຜິດຊອບໂຄງການຂັ້ນສູນກາງ ແລະ ຫ້ອງຖິ່ນກໍ່ໄດ້ມີການປຶກສາຫາລືຜ່ານມາແລ້ວ ໂດຍສະເພາະກໍ່ແມ່ນແຜນການບໍລິຫານຄຸ້ມຄອງສິ່ງກໍ່ສ້າງຂອງໂຄງການ ແລະ ແຜນການນຳໃຊ້ທ່າເຮືອຖາວອນຂອງສະມາຊິກກຸ່ມເຮືອນຳທ່ຽວ, ຜ່ານການປຶກສາຫາລື ລະຫວ່າງ ເຈົ້າຂອງໂຄງການ ແລະ ຜູ້ໃຫ້ທຶນແມ່ນໄດ້ມີການປັບປຸງແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນມີດັ່ງນີ້:

- 1) ທາງໂຄງການເຫັນດີໃຫ້ການສະໜັບສະໜູນ ຕໍ່ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນຂອງໂຄງການ ຢູ່ເຂດໂສກປາແກ້ວ-ໂສກໄທດຳ, ມີລວງຍາວໜ້ານ້ຳປະມານ400ແມັດ ທີ່ສາມາດຈອດເຮືອໄດ້ 60-80ລຳ
- 2) ບາງໜ້າວຽກ ຈະໄດ້ຕັດຮອນອອກເຊັ່ນ: ເສັ້ນທາງລົງຫາທ່າເຮືອ, ທາງຍ່າງທຸ່ນລອຍ ແລະ ອື່ນໆ ເພື່ອໃຫ້ແທດເໝາະກັບຕົວເລກກົງປະມານທີ່ໂຄງການສະໜັບສະໜູນປະມານ 100,000\$ ລົງມາ.
- 3) ອີງໃສ່ໄລຍະເວລາຂອງໂຄງການ ທາງທີມງານຊຽວຊານຈະໄດ້ຮິບຮ້ອນດຳເນີນວຽກງານການຈັດຊື້-ຈັດຈ້າງ ເພື່ອໃຫ້ແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນໄດ້ສຳເລັດໃນໄລຍະການມອບ-ຮັບໂຄງການ.

ເພື່ອໃຫ້ຜູ້ເຂົ້າຮ່ວມໄດ້ປະກອບການໄດ້ແລກປ່ຽນຄຳຄິດເຫັນ, ຜ່ານການຄົ້ນຄ້ວາປຶກສາຫາລື ກອງປະຊຸມກໍ່ໄດ້ມີຄວາມເປັນເອກະພາບກັນດັ່ງນີ້.

1. ກອງປະຊຸມເຫັນດີເປັນເອກະພາບ ຕໍ່ກັບແຜນການກໍ່ສ້າງທ່າເຮືອຖາວອນຂອງໂຄງການ ຢູ່ໂສກປາແກ້ວ-ໂສກໄທດຳ ຕາມແຜນທີ່ໄດ້ມີການສຳຫລວດອອກແບບໃນໄລຍະຜ່ານມາ, ຊຶ່ງສະມາຊິກກຸ່ມເຮືອນຳ ທ່ຽວທັງໝົດ ແມ່ນເຫັນດີເປັນເອກະພາບຈັດຕັ້ງປະຕິບັດຕາມແຜນບໍລິຫານ ແລະ ຄຸ້ມຄອງສິ່ງກໍ່ສ້າງຂອງໂຄງການໃນການຈອດເຮືອຄື: ຕອນກາງຄືນແມ່ນເຫັນດີເປັນເອກະພາບກັນໄປຈອດຢູ່ທ່າເຮືອຖາວອນ, ສ່ວນຕອນກາງເວັນ ຈຶ່ງສາມາດນຳເຮືອມາບໍລິການນັກທ່ອງທ່ຽວ (ຫ້າມເດັດຂາດບໍ່ໃຫ້ມາຈອດຢູ່ບໍລິເວນຈຸດສາລາຊິມວິວ)
2. ພື້ນທີ່ຈະກໍ່ສ້າງທ່າເຮືອຖາວອນແມ່ນບໍ່ໄດ້ທັບຊ້ອນ ກັບພື້ນທີ່ເຊົ່າ-ສໍາປະທານທີ່ດິນຂອງບໍລິສັດ, ຊຶ່ງໃນໄລຍະຜ່ານມາທາງຫ້ອງການກ່ຽວຂ້ອງຂອງເມືອງ ກໍ່ໄດ້ລົງສຳຫລວດວັດແທກກຳນົດເຂດໄວ້ແລ້ວຄື:
 - ຈຸດທາງລົງທ່າເຮືອຖາວອນ(ທາງລົດໃຫຍ່) ແມ່ນກຳນົດເອົາຈຸດເຂດແດນເຊື່ອມຕໍ່ຂອງພື້ນທີ່ສໍາປະທານບໍລິສັດລາຊາໂຊກ ແລະ ທ່ານ ນາງ ວົງທອງ ຈາກເສັ້ນທາງເບ່ຕົງລົງໄປຫາອ່າງນ້ຳຼືມ ມີຄວາມຍາວປະມານ 800 ແມັດ, ກ້ວາງ 6 ແມັດ

- ຈຸດທາງຍ່າງລົງທ່າເຮືອຖາວອນ(ທາງຍ່າງ) ແມ່ນເລີ່ມຈາກທາງເປ່ຕິ່ງເຂດສາລາຕິກເປັດ(ຕາມເສັ້ນທາງ ຍ່າງເກົ່າ ມີຄວາມຍາວປະມານ 850ແມັດ, ກ້ວາງ 3-4 ແມັດ
 - ໃນຕໍ່ໜ້າ ຖ້າຫາກບໍລິສັດຜູ້ສຳປະທານທີ່ດິນ ມີແຜນດຳເນີນກິດຈະການເຮືອນຳທ່ຽວ ກໍ່ສາມາດມານຳ ໃຊ້ທ່າເຮືອຖາວອນຮ່ວມກັບສະມາຊິກກຸ່ມເຮືອນຳທ່ຽວໄດ້.
3. ມອບໃຫ້ຫ້ອງການຊັບພະຍາກອນທຳມະຊາດ ແລະ ສິ່ງແວດລ້ອມເມືອງແກ້ວອຸດົມ ໄດ້ກຳນົດແຜນຜັງ ລວມຂອງແຕ່ລະໂຄງການ ຢູ່ພື້ນທີ່ໂສກປາແກ້ວ-ໂສກໄທດຳ ແລະ ລວບລວມເອົາເອກະສານຕ່າງໆ ທີ່ທາງ ເມືອງໄດ້ອອກນິຕິກຳ ທີ່ຕິດພັນກັບພື້ນທີ່ກໍ່ສ້າງທ່າເຮືອຖາວອນ ເພື່ອເປັນຂໍ້ມູນອັນລະອຽດສະໜອງໃຫ້ ກັບໂຄງການ.
 4. ທາງພະແນກ ຖວທ ແຂວງວຽງຈັນ ຈະໄດ້ປະສານສົມທົບກັບພາກສ່ວນກ່ຽວຂ້ອງ ເພື່ອດຳເນີນການປະ ເມີນມາດຕະຖານເຮືອນຳທ່ຽວ ແລະ ການຈັດຕັ້ງເຜີຍແຜ່ ຂໍ້ຕົກລົງຂອງທ່ານເຈົ້າແຂວງວຽງຈັນ ວ່າດ້ວຍ ການຄຸ້ມຄອງກິດຈະກຳການທ່ອງທ່ຽວທາງນ້ຳ ໃຫ້ກັບຜູ້ປະກອບການເຮືອນຳທ່ຽວໄດ້ຮັບຊາບ ແລະ ປະຕິ ບັດ ກ່ອນການເປີດບໍລິການ-ການທ່ອງທ່ຽວຄືນໃໝ່ ຢູ່ອ່າງນ້ຳກຸ້ມ.
 5. ມອບໃຫ້ທີມງານຊຽວຊານປົກປ້ອງສັງຄົມ ສືບຕໍ່ປະສານສົມທົບກັບ ຄະນະຮັບຜິດຊອບໂຄງການຂັ້ນສູນ ກາງ ນຳສະເໜີຕໍ່ຜູ້ໃຫ້ທຶນ ເພື່ອອະນຸມັດງົບປະມານ ໃນການກໍ່ສ້າງທ່າເຮືອຖາວອນ ຢູ່ພື້ນທີ່ເປົ້າໝາຍ ທີ່ ໄດ້ມີການສຳຫລວດຮ່ວມກັນໃນໄລຍະຜ່ານມາ.
 6. ທາງຄະນະຮັບຜິດຊອບໂຄງການຂັ້ນສູນກາງ ຍັງຈະໄດ້ສືບຕໍ່ປະສານສົມທົບກັບ ຄະນະຮັບຜິດຊອບໂຄງ ການຂັ້ນແຂວງ ແລະ ທີມງານຊຽວຊານປົກປ້ອງສັງຄົມ ໃນການຈັດສັນ(ຈັດໂຊນ)ອາຄານຮ້ານຄ້າ ເພື່ອ ຄວາມເປັນລະບຽບຮຽບຮ້ອຍກ່ອນການມອບ-ຮັບໂຄງການ ໄປຕາມແຜນການບໍລິຫານຄຸ້ມຄອງ ແລະ ບຳ ລຸງຮັກສາຂອງໂຄງການກຳນົດໄວ້.

ກອງປະຊຸມໄດ້ປິດລົງເວລາ 16:00 ໂມງ ຂອງວັນດຽວກັນ ດັ່ງນັ້ນ, ຈຶ່ງເຮັດບົດບັນທຶກສະບັບນີ້ໄວ້ ເພື່ອເປັນ ບ່ອນອີງໃນການເຄື່ອນໄຫວຈັດຕັ້ງປະຕິບັດວຽກງານໃນຕໍ່ໜ້າ, 

ຫົວໜ້າພະແນກ ຖວທ ແຂວງວຽງຈັນ
(ປະທານກອງປະຊຸມ)



ສີສະຫວາດ ແສງພະຈິນ

ຜູ້ບັນທຶກ

